



Town of Amherst  
Regular Council Meeting  
Agenda

Date: **Monday, November 28, 2022**  
Time: **6:00 pm**  
Location: **Council Chambers, Town Hall**

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	Pages
<b>1. CALL TO ORDER</b>	
<b>2. TERRITORIAL ACKNOWLEDGMENT</b>	
"We [I] would like to begin by acknowledging that the land on which we gather is the traditional unceded territory of the Mi'kmaw Peoples."	
<b>3. APPROVAL OF AGENDA/MINUTES</b>	
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7.5.	Inter Municipal Tourism - No Report	
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7.8.	Northern Region Solid Waste - Baker	219 - 219
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7.10.	CJSMA Report - Emery	222 - 222

8. ADJOURNMENT

**Town of Amherst  
Public Hearing  
Minutes**

**Date:** October 20, 2022  
**Time:** 5:00 pm  
**Location:** Council Chambers, Town Hall

**Members Present** Mayor David Kogon  
Councillor George Baker  
Councillor Hal Davidson  
Councillor Lisa Emery  
Councillor Dale Fawthrop  
Councillor Leon Landry

**Members Absent** Deputy Mayor Christie

**Staff Present** Jason MacDonald, Chief Administrative Officer  
Andrew Fisher, Director of Planning & Strategic Initiatives  
Kim Jones, Director, Corporate Communications/Municipal Clerk  
Tom McCoag, Corporate Communications Officer  
Natalie LeBlanc, Deputy Clerk

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**1. Call to Order**

Mayor Kogon called the Public Hearing to order.

**2. Land Use Bylaw Amendment - Flemming/Paradise Rezoning Application**

**2.1 Presentation**

Mr. Fisher presented.

**2.2 Written Submissions**

There were no written submissions received.

**2.3 Public Comments**

John Gillis, 6 Mosher Street, stated he built his home from July to October 2011. He asked exactly where and what street this application pertained to. Mr. Fisher replied it was at the end Flemming Avenue if you turn right, behind Sandstone Crescent, as shown on the map included in the presentation. Mr. Gillis asked if this was in relation to the machinery clearing land off of Oceanview Drive, to which Mr. Fisher replied no.

Mark Morgan, 86 Willow Street, expressed his concern with future large apartment complexes being constructed in this area. He noted there is a large parcel of land here currently for sale, and that he feels the application for Brown/Mosher is just the beginning, that this area could also have potential for large apartment complexes to be constructed on them, which his property would then be surrounded by.

Andy Melvin, 1 Flemming Avenue, asked if the proposed development for this site was 4-units, to which Mr. Fisher replied yes, similar to the ones neighboring the property on Sandstone Crescent. For further clarity he asked if it's for one 4-unit, or four, to which Mr. Fisher replied it is for four, 4-units.

There were no further questions or comments from the public regarding this application.

**3. Development Agreement Application - Lot 2021-2R Brown/Mosher Street**

**3.1 Presentation**

Mr. Fisher presented.

**3.2 Written Submissions**

Mr. MacDonald read the written submissions for the record.

### **3.3 Public Comments**

Wayne Dickie, 32 Donald Avenue, asked about the walkway between his property and 34 Donald Avenue, stating he would like to purchase it so it could not be used as a public walkway. He indicated he has reached out to Mr. Fisher over the past two years about this however no action has been taken. Mr. MacDonald replied that the Town does not own this parcel of land, that it belongs to the applicant. Mr. Dickie asked if it will remain a walkway, to which Mr. MacDonald replied that the plans do not show a walkway, therefore it would remain vacant. Mr. Dickie asked if he could have that in writing, Mr. MacDonald replied it is part of the proposed development agreement. Mr. Dickie replied it is a big concern for him and his neighbor, and that he was told the Town owned it. Mr. Fisher replied the original intent for this parcel of land when Donald Avenue was developed was for a walkway, however it was never fully developed, and that it is owned by the applicant, but does not appear to be part of the proposed development. Mr. Dickie again asked for this in writing. Mr. Fisher replied the purpose of this meeting is for public feedback, and that Council will take into consideration that it has been expressed that the walkway is not wanted.

Evelyn Bradford, 3 Brown Street, noted she is pleased to see that the proposed development agreement does now include stop signs at Brown and Mosher. She expressed her concern with the increase in traffic this proposed development would create. She stated her son is in a wheelchair, which is why they bought in their current quiet neighborhood. She expressed concern for pedestrian safety in general, noting several seniors and dog walkers also use this area. She would like to see sidewalks constructed on Brown and Mosher, as there currently are none. She also is concerned with the limited parking being proposed, wondering where the overflow will go. If it overflows onto Brown and Mosher, she added this will make pedestrian safety even more dangerous.

Dave Smith, 34 Donald Avenue, also expressed concern with the walkway between his property and 32 Donald Avenue, stating there will be no privacy and he doesn't want people using it at all hours of the day and night. He is also concerned with where the snow that would be removed from the proposed development will go.

Mark Morgan, 86 Willow Street, noted he has lived at his residence for over a decade, and that he and his neighbors are all friends, who help each other out, celebrate special occasions together, share tools and even new recipes. He and his neighbors need help from Council, and because of Council. No one wants this. They are a small group of diverse people, Syrians, Mi'kmaq, African, Irish, and 2 LGBTQIA2S+, who support and know each other. This will not be the case if this development is approved, the residents will not know or care about him or his family, and unfortunately neither will he care about them as there will simply be too many people. He feels a development of this size will have a negative impact on these neighborhood relationships, that he will not know his neighbors, and it will give the area a "city mentality", which no one moved to Amherst for. He feels Council has already made up their minds, and therefore would like Council to consider naming the new road off of Willow with a Mi'kmaq/African Nova Scotian flare, do not say it is an extension of Gallagher as is done with so many other streets where one turns into another even though it's the same street, increase the crisis first response budget to anticipate and de-escalate situations as a result of such a large increase in population and freeze tax rates in the area until construction is complete, and pass it on to the building or be satisfied with the increase tax base when complete, and do not retroactively tack on the savings back to residents when construction is complete.

Judy Tooke, 18 Donald Avenue, indicated she has lived at her property for 42 years. Her biggest issue is with the potential for further flooding, which the Town has not addressed for years. Dickey Brook is overgrown, if there is another flood like this past February it will be awful. There is not enough overflow to put this proposed development here. Her and her neighbors know and support each other, and do not want these buildings constructed in this area.

William Nichols, 107 Willow Street, is not opposed to development but is opposed to this large development. He feels there are signs now of similar development going on in the Quarry field off of Willow Street. He expressed his concerns with traffic increase, which has not been addressed, and does not agree with the statistics given in the traffic study. He is glad there will be stop signs to slow traffic down, however still has other concerns with flooding and overflow, and the culverts not being adequate to address it. He gave an example of a sump pump at a home on Donald Avenue that runs all day long. He asked about the bump-out on Gallagher Street, and noted that nothing in the plan addresses what will be done with it, could it be addressed, will it be used for parking, building, or greenery, or will it be put for sale.

Helena Rudderham, 7 Rhodes Avenue, is concerned with putting what could be 500+ people in this small space and the impact all the way around. She learned this evening that emergency and education services are a provincially responsibility, and asked if emergency services and schools are provincial responsibilities, has the province been notified of this application, and if not why not. She feels it would be irresponsible for Council to permit this development. She expressed concern with climate change, and Dickey Brook, and the dykes that need to be fixed or we will all be under water. She is of the opinion this development cannot be justified, and where are the people are coming from as Amherst's population has remained the same for years. She asked why they cannot build such a large-scale development in a more appropriate area. She feels 4-units would fit better in this area. She added that "we", the residents are the town, and pay to run the town, and that this will increase taxes. She noted that several residents who could not attend this evening were also opposed, and that here tonight are only a small number. She asked what it would take for Council, our representatives, to reject this application.

John Gillis, 6 Mosher Street, quoted a fairly famous Canadian actor who stated the needs of many are outweighed by the needs of a few; he feels this is the case here. He stated he believes in compromise, and all things in moderation, but that this proposal flies in the face of that, it is just unreal. He suggested the people in power consider that instead of these 3 or 4 monsters that stick out ridiculously, they consider allowing only 4-units like the ones we have around town now that are beautiful. He would like to see one story, 4-units developed in this area rather than what is being proposed.

Teresa Nichols, 10 Donald Avenue, indicated she has lived at her residence for 21 years. She is concerned with flooding, and wildlife in the area that they enjoy. She has had three floods, the first one was only inches deep, the next one up to the furnace, but this last time up as high as three stairs, and remained there. The creek behind her property often looks like it will cross the flood plain during the winter thaw. She also expressed concerns with outdoor lighting from the proposed development, adding that the lighting from E.B. Chandler and over the bridge/culvert on Donald Avenue is already too bright. She would like to have them off at night however does not feel it is safe. She feels this development would affect the enjoyment of her property in general.

Dr. Keith Short, 9 Casper Court, stated that although he would not be directly affected by this development, he is concerned with the increase in traffic it would create along Willow Street. He feels the traffic study, done on June 20, did not provide a clear reflection of the amount of vehicular and pedestrian traffic travelling in this area during peak school times, and that Council request it be re-done, as it is not an accurate reflection of what happens on a day-to-day basis. Or Council should go stand on the corner in the morning when children are going to school, the speeds on Willow are not OK. He spoke to Mr. Fisher's point of provincial responsibilities, and to the massive crisis at the Cumberland Regional Hospital due to the shortage of doctors and nurses, and that if Council did not recognize this as a serious issue than they should not be on Council. The hospital and school system are not set up for such a large influx of people, and he feels this, along with the increase in traffic, needs to seriously be taken into account.

James King, 105 Willow Street, indicated he moved here only two months ago, and advised that he shares the single largest property line with this development. Coming from Ontario he is used to this kind of stuff, where things go up, people complain and nothing happens. He would like to know if the potential increase in crime that comes with such a development has been considered.

Jean Thompson, 49 Willow Street, also expressed concern with the increase in traffic this development would result in, and does not believe having three crosswalks between Spring Street and the high school is safe, adding people often park on the street at the Spring/Willow intersection further congesting this area. This is not safe, especially in the morning when the sun is shining brightly along this route. She asked if the increase in workload for police has been considered, has the Police Commission been notified of this application. She feels this property should remain as a greenspace, perhaps a park which would be lovely, but instead if this development is approved will now just be cement, which is too bad.

Jonathan McClelland, with the Cumberland Business Connector, stated he has no connection to this application. He spoke to the population in Amherst, in 1921 was 41,000 in Cumberland region, by 2016 it was 30,005, and increased in 2021 by 500, stating the population is growing. The biggest issue he sees everyday is the lack of workforce, due to the lack of housing. People are turning down jobs because there is no housing, which is part of the doctor and nursing shortage. People cannot afford to live in Moncton or Truro and travel due to gas prices. All businesses in Amherst are facing this challenge of housing shortages.

Connor Manson, the architect for this proposed development, spoke at this time, stating that he knows he is probably one of the only people in attendance that supports this development. He advised that he did not get into this business to ruin communities, but to enhance them. He advised those in attendance that he has heard their concerns, and that a lot of them are out of his control. He assured everyone that the buildings being proposed are state of the art, he can put noise concerns somewhat to rest, but things like potential flooding and increase in traffic are not in his control, as much as he wishes he could solve them. He is here to improve the community, and looks forward to the response from the Town, and continuing to work with them for this development.

Helena Rudderham took to the podium again, commenting that she appreciates the architect's comments, but disagreed in that this would ruin their neighborhood. She agreed that housing is needed, but not this type on this property. She does not want to solve a problem by compounding more.

Jeff Smith, 88 Willow Street, appreciated Mr. Manson's comments as well. However, he has concerns with the traffic study that was completed, was the date coincident. He is not opposed to residences being constructed, wants to see affordable housing, and wants to see the town grow, but responsibly, and feels that permitting this development in this area is incredibly irresponsible. He stated he works in IT and has seen companies grow too big, too quick and then crumble, he does not want to see this happen with our town. He wants the Town to push back on this development, and consider affordable housing instead. He feels this application is incredibly irresponsible, and jeopardizes the safety of the children in this area.

Darlene Daley, 105 Willow Street, noted her property is close to the new road being proposed off of Willow Street. She is concerned about this, and that if a sidewalk is constructed along this side of Willow Street, she will lose a portion of her front yard.

Mr. Dickie spoke again asked about the brook, and why is nothing being done, stating every time there is a flood it is a \$2,000 deductible for the property owner, which does not come off of their taxes. Mr. MacDonald replied the Town has received funding to improve storm water management in the area, and that the Town has taken other measures to clean the brook and culverts. Mr. Dickie also asked if Mr. McClelland lived anywhere near the proposed development, to which he replied no.

Stephen Coates noted he is part owner of the 50 acres of property on the other side of Willow Street across from the proposed development. He indicated he has 6 to 8 lots that have been for sale since 2009 or 2010, they put them on the market again this summer with the unit across from what used to be the old Tantramar Motors and never had one offer all summer. So, if this development is not approved, he knows where there is 50 acres that is open. He spoke to his childhood growing up in this area on Willow Street, and the construction of the asphalt road and the school for the deaf, and that this could not be worse than that.

At this time Mayor Kogon thanked everyone for coming, assured them their concerns would be considered, and closed the public hearing.

#### **4. Adjournment**

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Kimberlee Jones  
Municipal Clerk

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David Kogon, MD  
Mayor

**TOWN OF AMHERST  
Regular Council Meeting  
Minutes**

**Date:** October 24, 2022  
**Time:** 6:00 pm  
**Location:** Council Chambers, Town Hall

**Members Present** Mayor David Kogon  
Deputy Mayor Sheila Christie  
Councillor George Baker  
Councillor Hal Davidson  
Councillor Lisa Emery  
Councillor Dale Fawthrop  
Councillor Leon Landry

**Staff Present** Jason MacDonald, Chief Administrative Officer  
Dwayne Pike, Police Chief  
Greg Jones, Director of Fire Services  
Aaron Bourgeois, Director of Operations  
Andrew Fisher, Director of Planning & Strategic Initiatives  
Sarah Wilson, Director of Finance  
Krista Crossman, Director, HR & Customer Services  
Kim Jones, Director, Corporate Communications/Municipal Clerk  
Tom McCoag, Corporate Communications Officer  
Natalie LeBlanc, Deputy Clerk

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**1. CALL TO ORDER**

Mayor Kogon called the meeting to order at 6:00 p.m.

**2. TERRITORIAL ACKNOWLEDGMENT**

Mayor Kogon gave the territorial acknowledgement.

**3. APPROVAL OF AGENDA/MINUTES**

**3.1 Approval of the Agenda**

Moved By Councillor Emery  
Seconded By Councillor Fawthrop  
To approve the agenda as circulated.

**Motion Carried**

**3.2 Approval of Minutes**

**3.2.1 September 26, 2022 Special Council**

Moved By Councillor Landry  
Seconded By Councillor Baker  
To approve the minutes of the September 26, 2022 special meeting  
of Council as included in the agenda package.

**Motion Carried**

**3.2.2 September 28, 2022 Council**

Moved By Deputy Mayor Christie  
Seconded By Councillor Emery  
To approve the minutes of the September 28, 2022 regular meeting  
of Council as included in the agenda package.

**Motion Carried**

**3.2.3 October 13, 2022 Special Council**

Moved By Councillor Baker  
Seconded By Councillor Fawthrop  
To approve the minutes of the October 13, 2022 special meeting of  
Council as included in the agenda package.

**Motion Carried**

**4. REQUESTS FOR DECISION**

**4.1 Election of Deputy Mayor**

Deputy Mayor Christie nominated Councillor Landry. Councillor Fawthrop seconded this motion.

Councillor Emery nominated Councillor Baker. There was no seconder, and Councillor Baker expressed he was not interested in the position.

Councillor Baker nominated Councillor Emery. There was no seconder.

There being no further nominations, Councillor Landry was elected Deputy Mayor for the November 1, 2022 to October 31, 2023 term.

**4.2 Council Committee Appointments**

**Moved By Councillor Emery**

**Seconded By Councillor Landry**

**That Council approve the following committee appointments effective November 1, 2022 to October 31, 2023**

**Mayor Kogon**

- Regional Emergency Management

**Councillor Davidson**

- Planning Advisory Committee
- Inclusion, Diversity and Equity Committee

**Councillor Baker**

- CJSMA
- Northern Region Solid Waste Committee

**Councillor Emery**

- CJSMA
- Municipal Alcohol Project
- Regional Emergency Management
- Senior Safety Advisory Group
- Tyndal Wellfield Advisory Committee
- Inclusion, Diversity and Equity Advisory Committee
- Intermunicipal Tourism Committee

**Councillor Fawthrop**

- Cumberland Public Libraries Board
- LA Animal Shelter
- Tree Advisory Committee
- Tyndal Wellfield Advisory Committee
- YMCA Board of Directors
- Accessibility Advisory Committee

**Councillor Landry**

- Intermunicipal Poverty Reduction Committee
- Planning Advisory Committee
- Accessibility Advisory Committee

**AND FURTHER, that Councillor Baker be appointed to the Amherst Board of Police Commissioners for a 3 year term, expiring October 31, 2025, and for clarity that Councillor Davidson's appointment to the Amherst Board of Police Commissioners expires October 31, 2023;**

**AND FURTHER, that all members of Council are also appointed to the Committee of the Whole of Amherst Town Council and the Town of Amherst Audit Committee.**

**Motion Carried**

**4.3 Dr. & Mrs. H.E. Christie Foundation Donation**

**Moved By Deputy Mayor Christie**

**Seconded By Councillor Landry**

**That Council approve the request to receive a donation of \$12,000 from the Dr. & Mrs. H.E. Christie Community Foundation to fund a corresponding grant of \$12,000 to the Cumberland County Minor Hockey Association.**

**Motion Carried**

- 4.4 **Flag Protocol Policy Amendment**  
**Moved By Councillor Davidson**  
**Seconded By Deputy Mayor Christie**  
**That Council approve of amendments to the Flag Protocol Policy 02300-04.**

**Motion Carried**

**5. INTERNAL COMMITTEE REPORTS**

- 5.1 **Planning Advisory Committee - No Report**  
Information item only; no direction given or action required.
- 5.2 **Amherst Board of Police Commissioners**  
Information item only, no direction given or action required.
- 5.3 **Audit Committee - No Report**
- 5.4 **Amherst Youth Town Council**  
Councillor Davidson would like to know why there has been no representation at Council meetings, and wants this to be followed up on immediately.
- 5.5 **Accessibility Advisory Committee**  
Deputy Mayor Christie asked about the repairs to the elevator at the stadium. The CAO replied parts have been order. She would like the Accessibility Committee to look at options and a longer-term solution if it can not be repaired soon. Councillor Landry will have this item placed on the agenda for the next Accessibility Committee meeting.
- 5.6 **Inclusion Diversity and Equity Committee - Emery**  
Information item only; no direction given or action required.

**6. EXTERNAL COMMITTEE REPORTS**

- 6.1 **Cumberland Public Libraries - Fawthrop**  
Information item only; no direction given or action required.
- 6.2 **Cumberland YMCA – Fawthrop**  
Information item only; no direction given or action required.
- 6.3 **Northern Region Solid Waste Management - Baker**  
Information item only; no direction given or action required.
- 6.4 **L. A. Animal Shelter - Fawthrop**  
Information item only; no direction given or action required.
- 6.5 **Senior Safety – No Report**
- 6.6 **Inter Municipal Tourism – No Report**
- 6.7 **Poverty Reduction - Landry**  
Information item only; no direction given or action required.
- 6.8 **Municipal Alcohol Project - No Report**

Before the meeting adjourned, Mayor Kogon took a moment to thank and recognize Deputy Mayor Christie for her six years of service on Amherst Town Council.

- 7. ADJOURNMENT**  
**Moved By Deputy Mayor Christie**  
**Seconded By Councillor Landry**  
**To adjourn the meeting.**

**Motion Carried**

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Kimberlee Jones  
Municipal Clerk

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David Kogon, MD  
Mayor

**Amherst Town Council  
Special Meeting  
Minutes**

Date: November 9, 2022  
Time: 4:00 pm  
Location: Council Chambers, Town Hall

Members Present Mayor David Kogon  
Councillor George Baker  
Councillor Hal Davidson  
Councillor Lisa Emery  
Councillor Dale Fawthrop

Members Absent Councillor Leon Landry

Staff Present Jason MacDonald, Chief Administrative Officer  
Kim Jones, Director, Corporate Communications/Clerk  
Sarah Wilson, Director of Finance  
Tom McCoag, Corporate Communications Officer  
Natalie LeBlanc, Deputy Clerk

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**1. CALL TO ORDER**

Mayor Kogon called the meeting to order at 4:00 p.m.

**2. REQUEST FOR DECISION**

**2.1 Town of Amherst Consolidated Financial Statements**

**Moved By Councillor Emery**

**Seconded By Councillor Fawthrop**

**That Council accept the recommendation of the Audit Committee to approve the Town of Amherst Consolidated Financial Statements for the year ended March 31, 2022, which have been audited by the firm Jorgensen & Bickerton Inc.**

**Motion Carried**

**3. ADJOURNMENT**

**Moved By Councillor Davidson**

**Seconded By Councillor Baker**

**To adjourn the meeting.**

**Motion Carried**

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Kimberlee Jones  
Municipal Clerk

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David Kogon, MD  
Mayor

# SYNOPSIS

## Second Reading Land Use Bylaw Amendment – Rezoning

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The subject vacant property is approximately 2.4 acres in area with frontage on Paradise Avenue, and is part of a larger vacant property known as Coates' Field. Development in the immediate area consists of single detached dwellings and four-unit townhouses. Changing the zoning of this property would allow multi-unit dwellings such as four-unit townhouses.

Following a Public Participation Opportunity held on July 20, 2022, the Planning Advisory Committee recommended that Council rezone the subject property. Council gave First Reading on September 28, 2022 and held an advertised Public Hearing on October 20, 2022.

### **MOTION:**

**That Council give second and final reading to an amendment to the Land Use Bylaw Schedule 'A' Zoning Map, by changing the zoning of a portion of PID 25001926 located on Paradise Avenue from Low Density Residential to General Residential.**



**AMHERST TOWN COUNCIL**

**RFD# 2022081**

**Date: November 28, 2022**

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**TO:** Mayor Kogon and Members of Council

**SUBMITTED BY:** Andrew Fisher, Director of Planning & Strategic Initiatives

**DATE:** November 28, 2022

**SUBJECT:** Land Use Bylaw Amendment – rezoning property on Flemming & Paradise Avenue

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**ORIGIN:** Application to amend the Land Use Bylaw Schedule “A” Zoning Map to rezone a 9,680 sq.m. (2.4 acre) portion of vacant property (PID 25001926), located at Fleming Street and Paradise Avenue from Low Density Residential to the General Residential Zone.

**LEGISLATIVE AUTHORITY:** *Municipal Government Act* Part VIII: Planning and Development.

**RECOMMENDATION:** That Council give second reading to amend the Land Use Bylaw Schedule 'A' Zoning Map by changing the zoning of a portion of PID 25001926 located at Fleming Street and Paradise Avenue from Low Density Residential to General Residential.

**BACKGROUND:** The subject vacant property is approximately 9,680m<sup>2</sup> (2.4 acres) in area with frontage on Paradise Avenue where Flemming and Paradise intersect. The subject property is part of a larger vacant property known locally as Coates’ Field. Development in the immediate area consists of single detached dwellings to the north along Fleming Avenue, and four-unit townhouses to the west on Paradise Avenue and Sandstone Crescent, which are zoned General Residential.

Excerpts from the Land Use Bylaw for the Low Density and General Residential Zones are provided in the attached Staff report to the PAC. The applicant has expressed an interest in developing 4-unit townhouses on the subject property, similar to those located to the west. Doing so would require that the zoning be changed to the General Residential Zone that allows a full range of residential types. Attached is a preliminary plan showing how four, 4-unit townhouses could fit on individual properties and meet all minimum requirements of the General Residential Zone.

**DISCUSSION:** As detailed in the attached staff report to the PAC, Staff feel this rezoning request conforms to general intent of the MPS and other regulations, and does not represent a significant potential impact on the surrounding area over and above the current situation.

**FINANCIAL IMPLICATIONS:** No direct costs related to this issue. Ongoing tax revenue once development is complete.





## AMHERST TOWN COUNCIL

RFD# 2022081

Date: November 28, 2022

**SOCIAL JUSTICE IMPLICATIONS:** Additional housing options are needed in the community.

**ENVIRONMENTAL IMPLICATIONS:** None specific to this issue.

**COMMUNITY ENGAGEMENT:** An advertised Public Participation Opportunity was held on July 20, 2022. The only members of the public in attendance were the applicant and the two current property owners. Following First Reading, an advertised Public Hearing was held on October 20, 2022. Beyond some questions of clarification, no substantial opposition to the rezoning was heard. Draft minutes of this hearing are part of the Council agenda for review and approval.

Should Council give second reading, a notice will be placed in a local newspaper advertising the 14-day appeal period.

**ALTERNATIVES:** 1) Do not give 2<sup>nd</sup> Reading; 2) Refer the application back to the PAC for more information.

**ATTACHMENTS:** 1) Preliminary Plan; 2) October 21/22 Staff Memo; 3) Staff report to PAC

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Report prepared by: A.Fisher

Report and Financial approved by:



# MEMO

**TO:** Planning Advisory Committee

**FROM:** Andrew Fisher, Director of Planning & Strategic Initiatives

**DATE:** September 6, 2022

**RE:** **Land Use Bylaw Zoning Map Amendment – Fleming Avenue**

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**ISSUE:**

An application to amend the Land Use Bylaw Zoning Map to change the zoning designation of a portion of PID 25001926 located at Flemming Street and Paradise Avenue from Low Density Residential to General Residential.

**BACKGROUND INFORMATION:**

The subject vacant property is approximately 9,680m<sup>2</sup> (2.4 acres) in area. As shown on the attached sketch, it has frontage on Paradise Avenue where Flemming and Paradise intersect. The subject property is part of a larger vacant property known colloquially as Coates' Field. Development in the immediate area consists of single detached dwellings to the north along Fleming Avenue, which is located in the Low Density Residential Zone. Four-unit townhouses exist to the west on Paradise Avenue and Sandstone Crescent, which are zoned General Residential.

Attached are excerpts from the Land Use Bylaw (LUB) showing the permitted uses for the General Residential and Low Density Residential Zones.

The Low Density Residential Zone, applied to mostly vacant lands at the northern end of town, permits a range of residential use limited to Single Detached, Double Dwellings and a few accessory uses such as home occupations and B&Bs. Most importantly, this zone does not permit multi-unit dwellings.

The applicant has expressed an interest in developing 4-unit townhouses on the subject property, similar to those located to the west. Doing so would require that the zoning be changed to the General Residential Zone that allows a full range of residential types. Attached is a preliminary plan showing how four, 4-unit townhouses could fit on individual properties and meet all minimum requirements of the General Residential Zone.

**PUBLIC PARTICIPATION OPPORTUNITY:** An advertised Public Participation Opportunity was held on July 20, 2022. The only members of the public in attendance were the applicant and the two current property owners.

## **RELEVANT POLICY AND DISCUSSION:**

The following MPS polices are relevant to this issue.

### General Residential Zone

RP-3 It shall be the intention of Council to include in the Land Use Bylaw a General Residential zone. This zone shall permit a range and density of residential development subject to the policies of this Plan and regulations of the Land Use Bylaw. Council shall zone all areas designated Residential to the General Residential Zone, except where other specific zones are applied.

### Low Density Residential Zone

RP-4 It shall be the intention of Council to include in the Land Use Bylaw a Low Density Residential Zone. This zone shall permit low density residential development subject to the policies of this Plan and regulations of the Land Use Bylaw. This zone will be applied to areas designated Residential where Council feels medium and high density residential development is inappropriate.

### Amendment Criteria

A-5 It shall be the intention of Council, when considering an amendment to this or any other planning document, including the entering into or amendment of a development agreement, to consider the following matters, in addition to all other criteria set out in the various policies of this planning strategy:

- (a) That the proposal conforms to the general intent of this plan and all other municipal bylaws and regulations.
- (b) That the proposal is not premature or inappropriate by reason of:
  - (i) the financial capability of the Town to absorb any costs relating to the development;
  - (ii) the adequacy of municipal water, sanitary sewer and storm sewer services;
  - (iii) the adequacy of road networks, in, adjacent to, or leading to the development;
- (c) That consideration is given to the extent to which the proposed type of development might conflict with any adjacent or nearby land uses by reason of:
  - (i) type of use;
  - (ii) height, bulk and lot coverage of any proposed building;
  - (iii) parking, traffic generation, access to and egress from the site;
  - (iv) any other matter of planning concern outlined in this strategy.

In review of RP-3 and RP-4 above that create the residential zones, the only significant difference between them is that the Low Density zone has been applied to areas where medium and high density development is considered 'inappropriate'. However, there are several examples where medium density townhouse developments have been mixed among detached dwellings. The two building types do not conflict with each other.

## **CONCLUSIONS:**

Staff feel this rezoning request conforms to general intent of the MPS and other regulations and does not represent a significant potential impact on the surrounding area over and above the current situation.

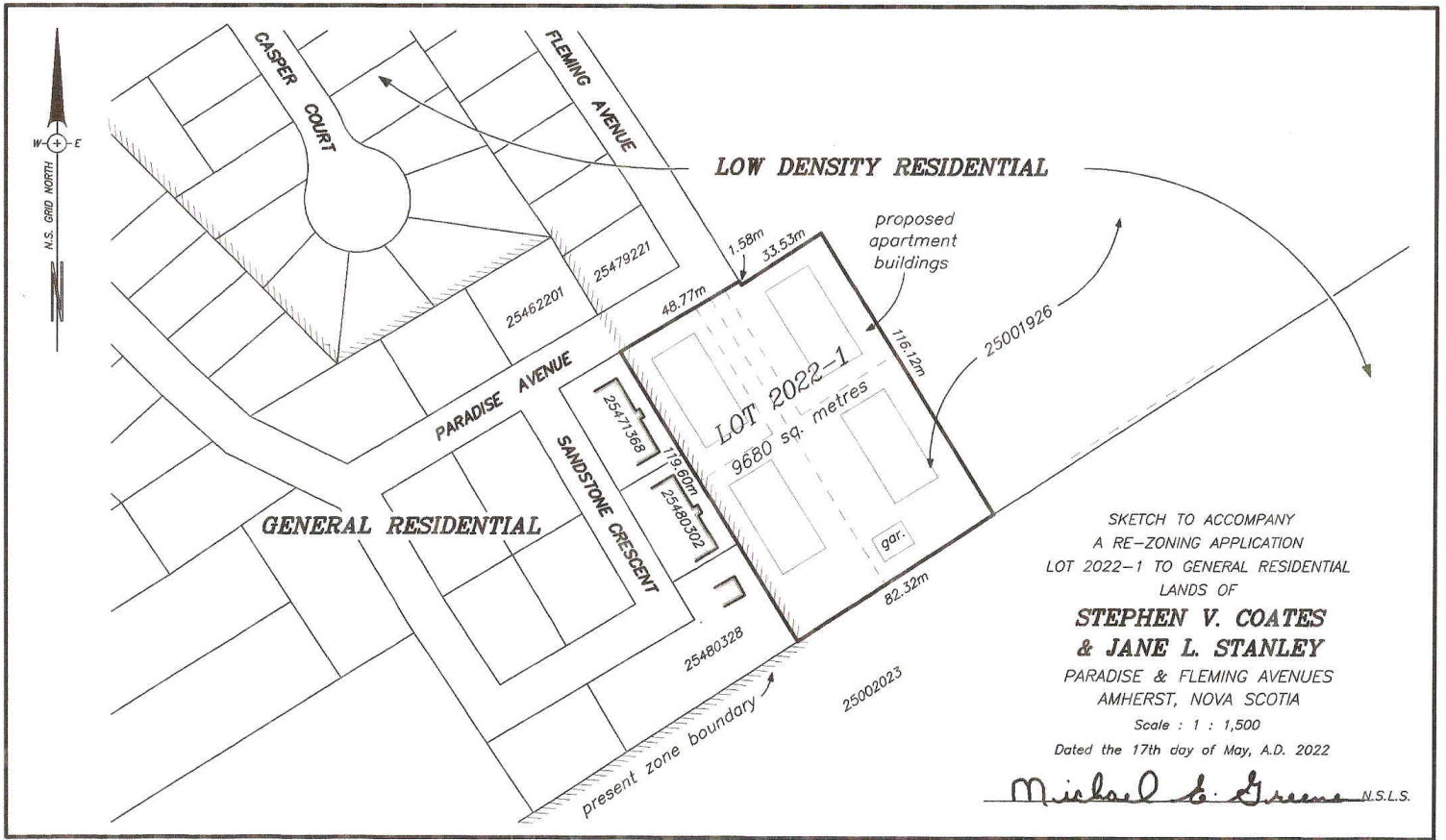
**MOTION OPTIONS:**

Option One: I move that the Planning Advisory Committee recommend that Council amend the Land Use Bylaw Schedule 'A' Zoning Map by changing the zoning of a portion of PID 25001926 located at Flemming Street and Paradise Avenue from Low Density Residential to General Residential.

Option Two: I move that the Planning Advisory Committee recommend that Council **not** amend the Land Use Bylaw Schedule 'A' Zoning Map by changing the zoning of a portion of PID 25001926 located at Flemming Street and Paradise Avenue from Low Density Residential to General Residential.

Option Three: I move that the Planning Advisory Committee defer the application and instruct Staff to provide more information.

**STAFF RECOMMENDATION: Option One.**



SKETCH TO ACCOMPANY  
 A RE-ZONING APPLICATION  
 LOT 2022-1 TO GENERAL RESIDENTIAL  
 LANDS OF

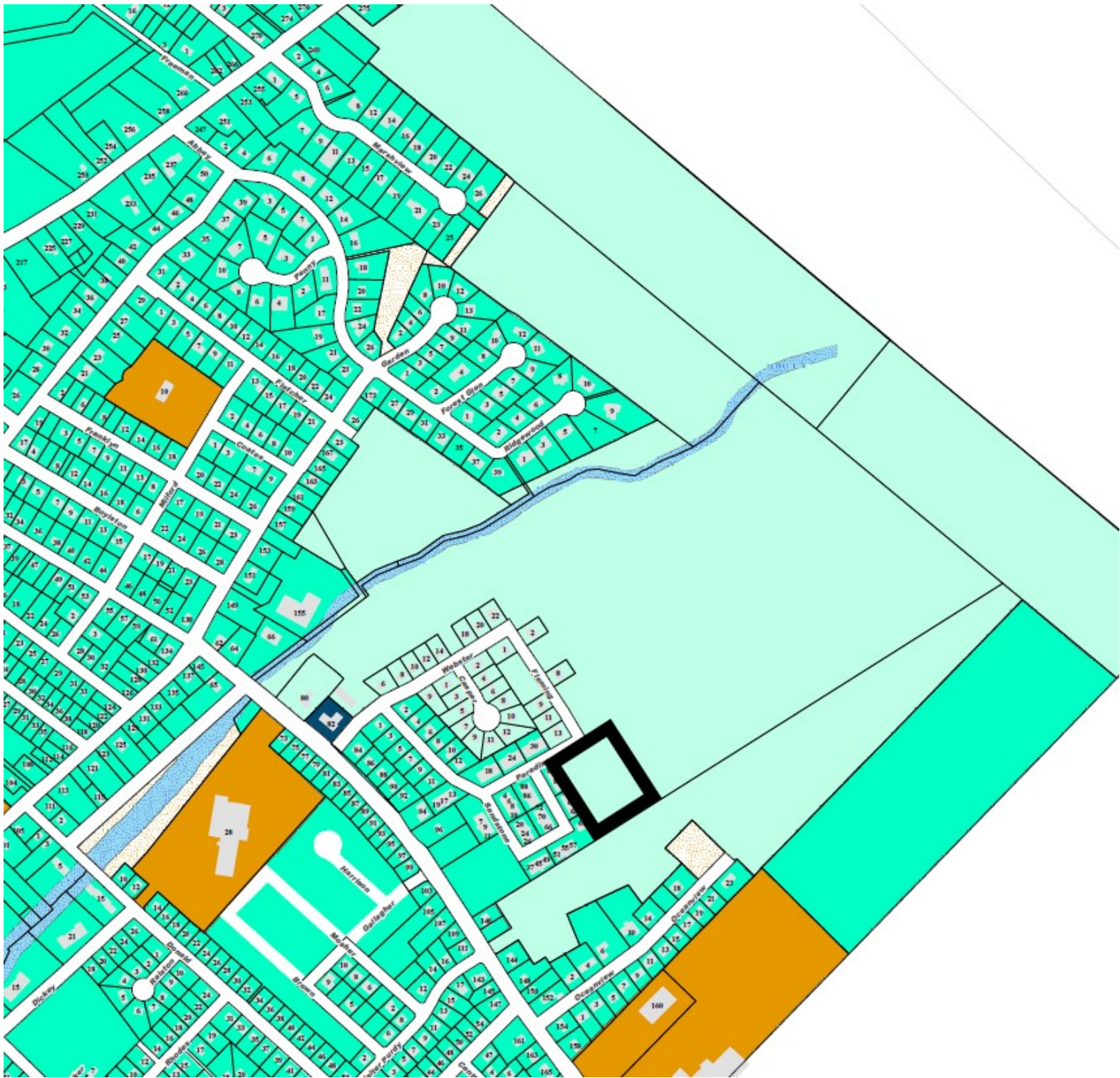
**STEPHEN V. COATES  
 & JANE L. STANLEY**

PARADISE & FLEMING AVENUES  
 AMHERST, NOVA SCOTIA

Scale : 1 : 1,500

Dated the 17th day of May, A.D. 2022

*Michael G. Greene* N.S.L.S.



## **7.2 General Residential Zone**

### **7.2.1 General Residential Zone Uses Permitted**

In the General Residential Zone, the following uses shall be permitted:

- (a) Single Detached Dwellings
- (b) Double Dwellings
- (c) Apartment Buildings (less than 5 units)
- (d) Converted Dwellings (subject to section 7.1.3)
- (e) Townhouses
- (f) Boarding houses
- (g) Bed and Breakfast Establishments
- (i) Home Occupations
- (j) Residential Day Care Centres
- (k) Parks, Open Spaces, and Public Trails
- (l) Existing trucking operation at 76 East Pleasant Street (PID 25026626)

### **7.2.2 Permitted Uses by Development Agreement**

In a Residential Zone, an application for the following uses shall be considered by development agreement in accordance with the applicable Municipal Planning Strategy (MPS) Policy:

- (a) A restaurant as part of bed and breakfast establishment in accordance with Policy RP-16 of the MPS.
- (b) A group home or boarding house in accordance with Policy RP-13 of the MPS.
- (c) A garden suite in accordance with Policy RP-14 of the MPS.
- (d) Apartment buildings greater than 4 units in accordance with Policy RP-9 of the MPS.
- (e) Long Term Care Facilities and Homes for Special Care in accordance with Policy RP-19.

### 7.2.3 Zone Requirements

In any Residential Zone, all development permits shall be issued in conformity with the following requirements:

<b>(a) Single Detached Dwelling</b>		
Minimum lot area		450 m <sup>2</sup>
Minimum lot frontage		15 m
Minimum front yard		6 m
Minimum rear yard		6 m
Minimum side yard	One side	1.3 m
	Other side	3 m
Minimum flanking yard		6m
Maximum height		11 m

<b>(b) Multiple Unit Dwellings</b>		
Minimum lot area	Apartment Building	300 m <sup>2</sup> per unit
	Townhouse	300 m <sup>2</sup> per unit
	Converted/Duplex/Semi-Detached/Other	560 m <sup>2</sup>
Minimum lot frontage	Apartment Building	10 m per unit
	Townhouse	6 m per unit
	Converted/Duplex/Semi-Detached/Other	20 m
Minimum front yard	Townhouse	10 m
	All Other Types	6 m
Minimum rear yard	All Types	8 m
Minimum side yard	Apartment Building	One side 2 m
		Other side 3 m
	Townhouse	Common Wall 0 m
		Outside Wall 3 m
Minimum flanking yard		6 m
Maximum height		11 m

#### 7.2.4 Special Requirement - Existing trucking operation at 76 East Pleasant Street (PID 25026626)

- Any ground surface not covered by vegetation to be treated so as to be dust free;
- No storage aggregate (sand or gravel) be permitted on the property;
- The storage of petroleum products on the site be in compliance with any and all relevant provincial regulations.

For the purposes of this section the existing trucking operation includes: the parking, storage and maintenance of up to four commercial motor vehicles; the parking, storage and maintenance of up to 4 additional passenger vehicles associated with the business; the operation of an office within the existing single detached dwelling on the property used to coordinate the day to day operation of the business as well as usual bookkeeping and business functions; the storage of various hand tools and maintenance equipment; the use of the existing garage on the property for the trucking business; up to six persons employed on the site.

## 7.3 Low Density Residential Zone

### 7.3.1 Low Density Residential Zone Uses Permitted

In the Low Density Residential Zone, the following uses shall be permitted:

- (a) Single Detached Dwellings
- (b) Double Dwellings
- (c) Bed and Breakfast Establishments
- (d) Home Occupations
- (e) Residential Day Care Centres
- (f) Parks, Open Spaces, and Public Trails

### 7.3.2 Permitted Uses by Development Agreement

In a Residential Zone, an application for the following uses shall be considered by development agreement in accordance with the applicable Municipal Planning Strategy (MPS) Policy:

- (a) A restaurant as part of bed and breakfast establishment in accordance with Policy RP-16 of the MPS.
- (b) A garden suite in accordance with Policy RP-14 of the MPS.

### 7.3.3 Zone Requirements

In any Residential Zone, all development permits shall be issued in conformity with the following requirements:

<b>(a) Single Detached Dwelling</b>		
Minimum lot area		450 m <sup>2</sup>
Minimum lot frontage		15 m
Minimum front yard		6 m
Minimum rear yard		6 m
Minimum side yard		1.3 m
		3 m
Minimum flanking yard		6m
Maximum height		11 m

<b>(b) Two Unit Dwellings</b>		
Minimum lot area		360 m <sup>2</sup> per unit
Minimum lot frontage		24 m
Minimum front yard		6 m
Minimum rear yard		6 m
Minimum side yard		4 m
Minimum flanking yard		6 m
Maximum height		11 m



**Lot Size (Without Pole)**

- Lot #1 = 1,860 sq. meters**
- Lot #2 = 2,410 sq. meters**
- Lot #3 = 2,170 sq. meters**
- Lot #4 = 2,590 sq. meters**



**Lot Size (Without Pole)**

- Lot #1 = 1,860 sq. meters
- Lot #2 = 2,410 sq. meters
- Lot #3 = 2,170 sq. meters
- Lot #4 = 2,590 sq. meters

# SYNOPSIS

## Development Agreement – Mosher/Brown Apartment Complex – Second Reading

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The subject Development Agreement would permit the construction of three, 54-unit apartment buildings on Lot 21-2R, located south of E.B. Chandler School, and north of Brown and Mosher Streets.

Following an advertised Public Participation Opportunity held on January 26, 2022, the Planning Advisory Committee reviewed the proposal and the public input on February 7, 2022, and requested changes and additional information from the applicant. This request was based on Planning Strategy Policies that relate to traffic impacts, separation distances, parking provisions, use of fencing and landscaping, shadow casting, engineering analysis for servicing, and other measures. With regard to addressing and/or mitigating the potential negative impacts of the development, reasonable attempts have been made within the revised proposal and supporting information provided by the applicant to address these issues, and have been incorporated into the draft agreement.

On September 6, 2022 the Planning Advisory Committee recommended that Council enter into the draft development agreement for Lot 21-2R. Council gave First Reading at their regular meeting on September 28, 2022 and held an advertised public hearing on October 20, 2022. Several residents in the area of the proposed development expressed their opposition to the proposal.

The proposal meets the general intent of MPS policies. The development agreement process provided the public an opportunity to give input and changes to the proposal have been incorporated into the development that are detailed in the attached report.

### **MOTION:**

**That Council give second and final reading to a Development Agreement for Lot 21-2R that would permit the construction of three, 54 unit apartment buildings.**



**AMHERST TOWN COUNCIL**

**RFD# 2022080**

**Date: November 28, 2022**

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**TO:** Mayor Kogon and Members of Council

**SUBMITTED BY:** Andrew Fisher, Director of Planning & Strategic Priorities

**DATE:** November 28, 2022

**SUBJECT:** Development Agreement – Mosher/Brown Apartment Complex

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**ORIGIN:** An application for a development agreement to allow construction of three, 54-unit apartment dwellings on Lot 2021-2R, vacant property located south of E.B. Chandler School, and north of Brown and Mosher Streets.

**LEGISLATIVE AUTHORITY:** *Municipal Government Act* Part VIII Planning and Development.

**RECOMMENDATION:** That Council give second and final reading to enter into the attached Development Agreement for Lot 21-2R that would permit the construction of three, 54-unit apartment buildings.

**BACKGROUND:** An advertised Public Participation Opportunity was held on January 26, 2022. On February 7, 2022 the PAC requested that the applicant address some specific concerns about the proposal and provide additional information. On September 6, 2022 the PAC reviewed the applicant's response and made recommendation that Council give First Reading.

Council gave First Reading on September 28, 2022 and scheduled an advertised public hearing for October 20, 2022. At the hearing, several residents in the area of the proposed development attended to express their opposition to the development. Several written submissions were also received and read into the record.

Council is referred to the September 6, 2022 staff report to the PAC that contains details about the proposed development, input received through the January 26, 2022 Public Participation Opportunity, review of the additional studies and information provided by the applicant, and a review of the relevant MPS policies. Also attached is a November 21, 2022 memorandum that provides planning staff comments in relation to the general matters of concern expressed by the public at the October public hearing.

**DISCUSSION:** As detailed in the attached staff report to the PAC, the proposal meets the general intent of MPS policies. The development agreement process provided the public an opportunity to give input that is detailed in the attached information package. The PAC, after reviewing the material, requested additional information and changes that the applicant has for the most part provided. With regard to addressing and/or mitigating the potential negative impacts, reasonable attempts have been made to address these issues and have been incorporated into the draft agreement. The public had a second opportunity to provide input at



the October 20 public hearing, which Council have had an opportunity to consider that public input and have been provided a supplemental memorandum that, in the opinion of planning staff, provides context to the general areas of public concern.

It is recognized that the scale of the proposed development is cause for concern; however, there is no specific MPS policy that would prohibit or significantly reduce the proposal's scale. Policies that relate to traffic impacts, separation distances, parking provisions, use of fencing and landscaping, shadow casting, engineering analysis for servicing, and other measures have all been addressed. When compared to other urban centres, it is difficult to argue that 4-storey apartment buildings cannot be compatible with single-detached dwellings.

**FINANCIAL IMPLICATIONS:** No significant costs specific to this issue. Ongoing tax revenue upon completion of the development.

**SOCIAL JUSTICE IMPLICATIONS:** Additional housing options are needed in the community.

**ENVIRONMENTAL IMPLICATIONS:** The proposal is considered infill development and represent efficient use of land and does not require significant extension of municipal services. Stormwater management plans are required under the development agreement.

**COMMUNITY ENGAGEMENT:** January 26, 2022 Public Participation Opportunity, and October 20, 2022 Public Hearing. Draft minutes of the October 20, 2022 public hearing are included in the Council meeting agenda for approval. If second and final reading given, a notice of the right to appeal council's decision is placed in the local newspaper.

**ALTERNATIVES:** 1) Give Second Reading of the development with specific amendments; 2) Refer the application back to the PAC for more information; 3) Reject the application citing specific policies that are not met by the proposal.

**ATTACHMENTS:** 1) Development Agreement; 2) November Memo to Council; 3) Staff report to PAC.

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Report prepared by:

Report and Financial approved by:

This Agreement made this \_\_\_\_\_ Day of \_\_\_\_\_ 2022.

Between:

**NovaView Developments Ltd.** (owner of property located at Lot 2021-2R Walter Purdy Avenue [PID 25002122], hereinafter called the "Owner"),

of the one part, and

**The Town of Amherst** (a body corporate in the Province of Nova Scotia, hereinafter called the "Town"),

of the other part.

WHEREAS the Owner wishes to obtain permission pursuant to Policy RP-9 of the Municipal Planning Strategy of the Town of Amherst, to construct three 54-unit apartment dwellings on property located at Lot 2021-2R Walter Purdy Avenue (PID 25002122).

AND WHEREAS a condition of the granting of approval of Council is that the Owner enter into an Agreement with the Town;

AND WHEREAS the Council of the Town, at its meeting on the \_\_\_\_<sup>th</sup> Day of \_\_\_\_\_ 2022, approved the said Development Agreement, subject to the registered Owner of the land described herein entering into this Agreement;

AND WHEREAS the following Schedules shall be attached to and form part of this Agreement:

- (a) Schedule 'A' - Terms and Conditions
- (b) Schedule 'B' - Property Location Map
- (c) Schedule 'C' – Site Plan
- (d) Schedule 'D' – Building Elevation
- (e) Schedule "E" - Stormwater Management Plan

NOW THEREFORE THIS AGREEMENT WITNESSETH THAT in consideration of the granting by the Town of the Development Agreement requested by the Owner, the Owner agrees as follows:

- 1) That the Owner is the registered owner of the aforesaid Lands in the Town of Amherst, hereinafter called the "Lands". The aforesaid Lands are the only lands in the Town of Amherst to which this Agreement applies, and the Lands are illustrated in the plan shown on Schedule B attached.
- 2) That the Owner may construct three (3) Apartment Buildings, each with fifty-four (54) dwelling units on the said Lands, subject to Schedules A, B, C, and D attached.
- 3) Nothing in this Agreement shall exempt or be taken to exempt the Owner or any other person from complying with the requirements of any Bylaw of the Town applicable to the Property (other than the Land Use Bylaw to the extent varied by this Agreement) or any Provincial or Federal statute, act, or regulation.

- 4) Any failure of the Town to insist upon strict enforcement of any requirements or conditions contained in this Agreement shall not be deemed a waiver of any rights or remedies that the Town may have and shall not be deemed a waiver of any subsequent breach or default in the conditions or requirements contained in this Agreement.
- 5) Should the Owner fail to act in accordance with any aspect of this Agreement, the Town shall retain the right to discharge the Agreement upon 30 days notification and / or enter the property and conduct the required work. The cost of the said work will become a lien on the property tax bill.
- 6) The Town shall issue the necessary Development Permit for the development upon expiration of the appeal period specified for Development Agreements under Section 249 of the *Municipal Government Act*, as the same may be amended from time to time, or upon the withdrawal or dismissal of any appeal which may be taken.
- 7) The Agreement shall be binding upon the parties hereto and their heirs, executors, administrators, successors and assigns, and shall run with the land which is the subject of this Agreement until such time as it is discharged by the Town in accordance with Section 229 of the *Municipal Government Act*.

**SIGNED, SEALED AND DELIVERED**

In the presence of

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**THE TOWN OF AMHERST**

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David Kogon MD, Mayor

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Jason MacDonald, MCIP, LPP, CAO

**FOR THE OWNER**

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Mounir Daaboul  
NovaView Development Ltd.

## Schedule A

## Lot 2021-2R (PID 25002122) - Development Agreement

### Terms and Conditions:

#### 1.0 USE OF LAND AND BUILDINGS

- 1.1 The use of the property shall be limited to three (3) Apartment Buildings each with fifty-four (54) dwelling units for a total of one-hundred-sixty-two 162 dwelling units on property shown on Schedule 'B'.
- 1.2 Each Apartment Dwelling shall consist of no more than 4 levels and shall generally conform to the designs shown on Schedule 'D'. Minor variations to the architectural details and footprint of the dwellings may be permitted, to the satisfaction of the Development Officer. Such changes shall not be considered substantial.
- 1.3 The location of each Apartment Dwelling, driveway, parking area shall generally conform the Site Plan shown on Schedule 'C'.
- 1.4 A minimum of 1.25 parking spaces shall be provided for each dwelling unit for a total of 203 spaces on the Lands and shall be generally configured as shown on Schedule 'C'.
- 1.5 Should the provision of additional parking spaces be deemed necessary, such parking facilities shall be located no closer to adjacent properties that front onto Donald Avenue and Willow Street. Additional parking spaces shall be subject to the approval of the Development Officer and shall not be considered a substantial change to this agreement.
- 1.6 Prior to issuance of a Development Permit for any building, the Owner shall submit a detailed landscaping plan generally based on the Landscape Concept Plan shown on Schedule 'C'. The detailed landscaping plan shall include but not be limited to the following:
  - 1.6.1 Specify the type of treatment of all areas during all phases of the development. Areas of the Lands not part of initial phases must be kept as grass or otherwise treated so as to minimize dust.
  - 1.6.2 Placement of a privacy fence approximately 1.8 m (6 ft) in height in the general location and extent as shown on the Landscape Concept in Schedule 'C'. Fencing must be installed before the nearest parking area is used by residents.
- 1.7 The Owner shall be responsible for maintaining screened solid waste containment areas, generally in the locations shown on Schedule 'C'.
- 1.8 Paving of the driveways and parking areas shall be completed for each nearest corresponding Apartment Building before an Occupancy Permit is issued for that dwelling.
- 1.9 The Owner shall be responsible for access to and within the Lands in the following ways:
  - 1.9.1 Prior to issuance of a Development Permit for any dwelling, the Owner shall be responsible for the extension of Gallagher Street to intersect with Willow Street and shall be constructed with base gravel in accordance with the Town of Amherst Infrastructure Development Standards.
  - 1.9.2 The Gallagher Street extension to Willow Street shall be used as the primary construction access.

- 1.9.3 Prior to issuance of an Occupancy Permit for any dwelling, the Owner shall install an asphalt driving surface over the extension of Gallagher Street to Willow Street in accordance with Town of Amherst Infrastructure Development Standards.
- 1.9.4 Prior to issuance of an Occupancy Permit for any dwelling, the Owner shall install a concrete sidewalk along the easter side of Gallagher Street to Willow Street accordance with Town of Amherst Infrastructure Development Standards.
- 1.9.5 The Site Plan shown on Schedule 'C' and the internal road network shall be reviewed to ensure emergency and larger service vehicles can be accommodated, according to Transportation Association Canada and Town of Amherst Guidelines.

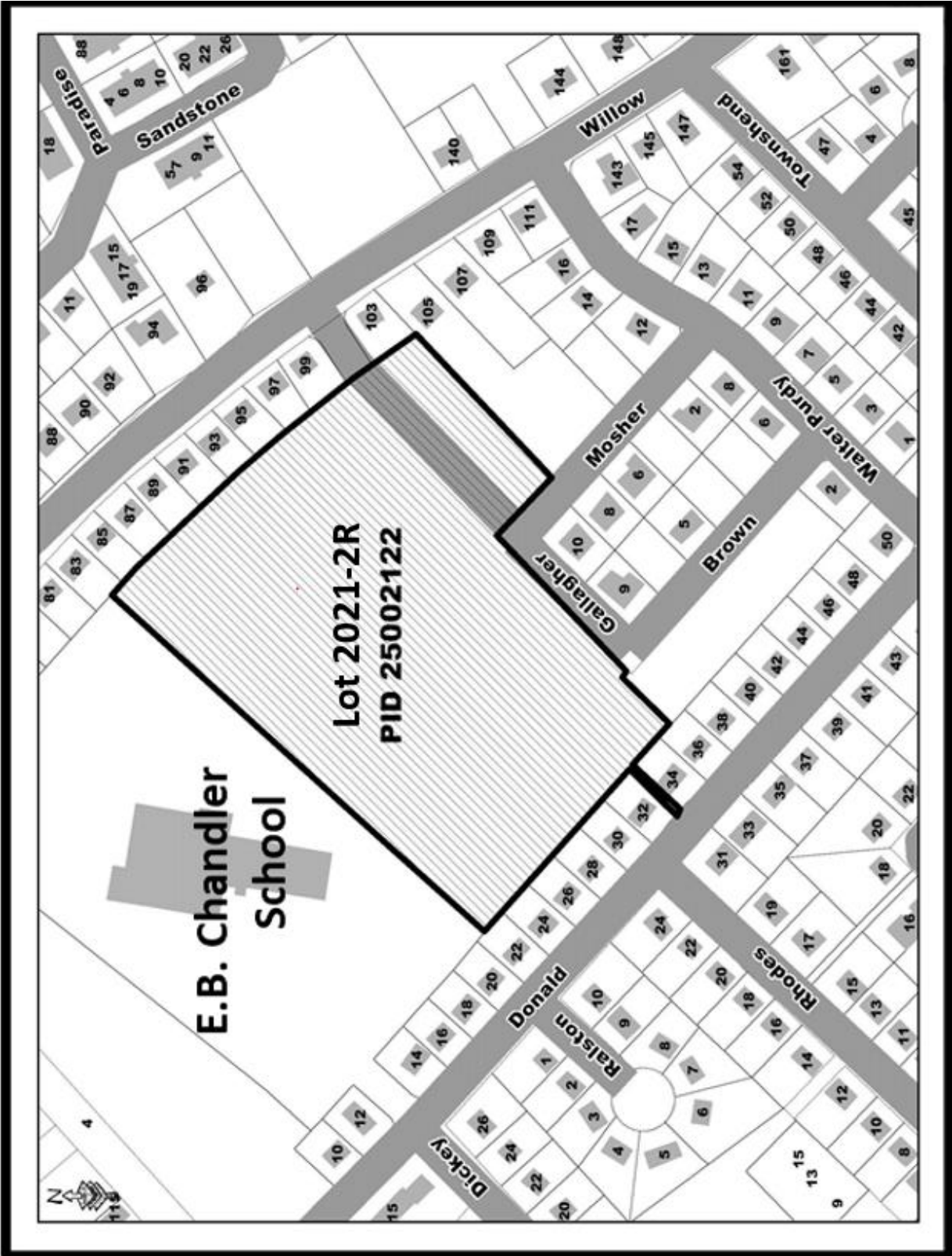
1.10 The Owner shall satisfy the Town of Amherst Infrastructure Development Standards provided as Schedule "F" of the Town of Amherst Subdivision Bylaw.

1.11 The Owner's Engineer shall revise the Stormwater Management Plan shown on Schedule "E" to reflect the revised location of Building C, and shall provide a design brief of the revised Stormwater Management Plan confirming that said Plan will not result in an increase of peak flows over existing conditions.

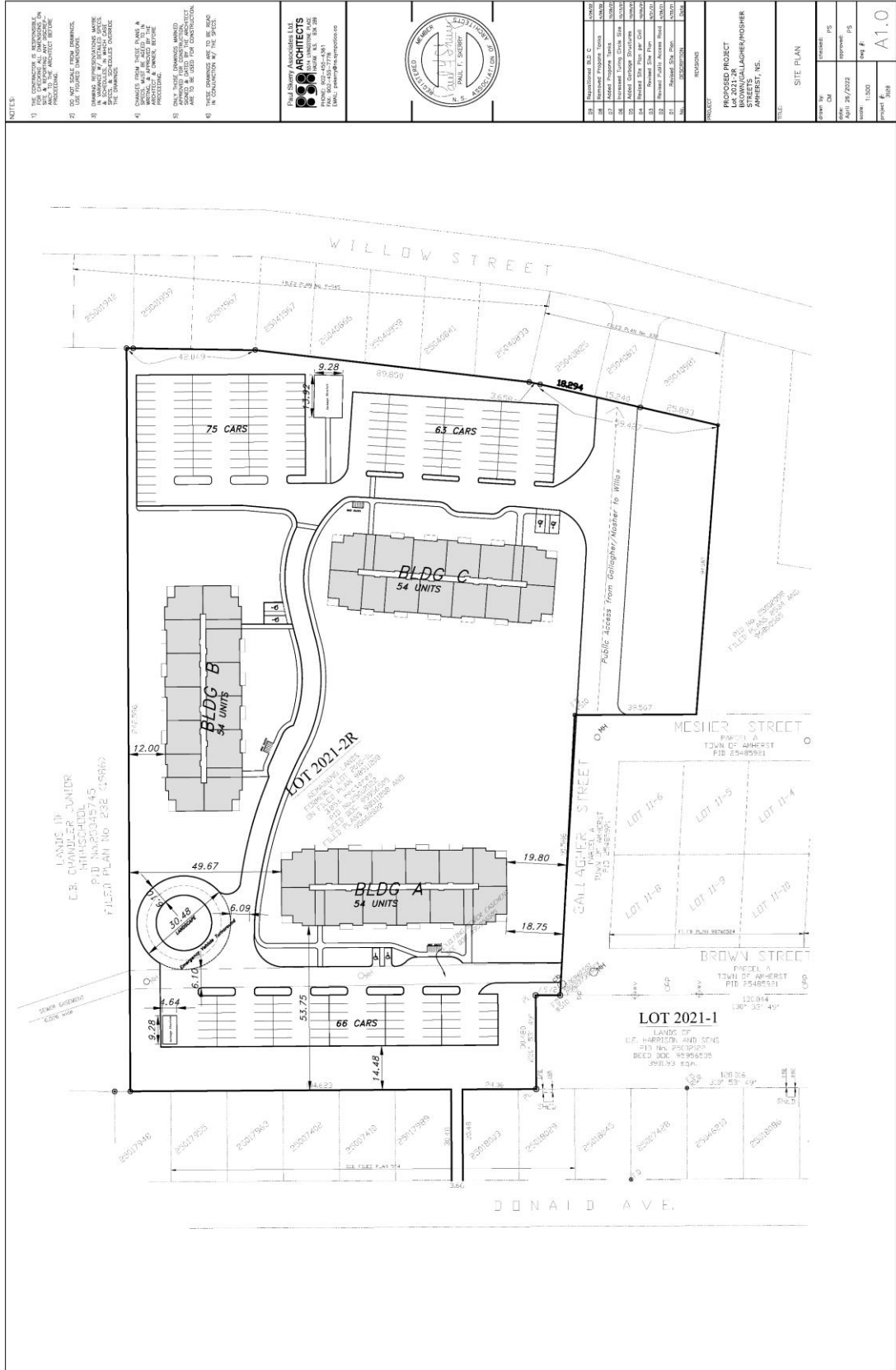
## 2. GENERAL REQUIREMENTS

- 2.1 The Owner shall keep the Lands and buildings and any portion thereof clean and in good repair. All elements of the development on the Lands shall be regularly maintained and kept in a tidy state, and free from unkept materials of any kind.
- 2.2 In addition to *Part 5 – Hours of Construction* under the Town of Amherst Building Bylaw D-6, operation of heavy equipment and electrical generators shall not take place on the property from 8:00 P.M. to 7:00 A.M.
- 2.3 Signage on the property shall conform to the Town of Amherst *Land Use Bylaw*.
- 2.4 The Owner shall ensure that exterior lighting does not shine directly onto adjacent properties.
- 2.5 Solid waste management shall be in conformance with the Town of Amherst *Solid Waste Bylaw*.
- 2.6 The Owner shall be responsible for storm water management during and after construction.
- 2.7 The Owner shall take all reasonable steps to maintain a clean worksite during construction by picking up building material waste, and taking all reasonable measures to minimize dust.
- 2.8 Accessory buildings may be permitted on the Lands in accordance with the Town of Amherst Land Use Bylaw.

**SCHEDULE "B"**



# SCHEDULE "C"





# SCHEDULE "D"



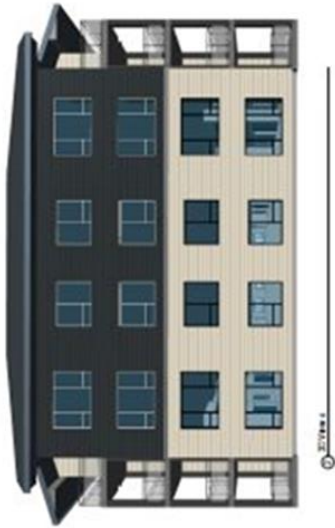
SECTION 1



SECTION 2

PRELIMINARY  
NOT FOR CONSTRUCTION

# SCHEDULE "D"



PRELIMINARY  
NOT FOR CONSTRUCTION



# MEMO

**TO:** Mayor Kogon and Members of Council

**FROM:** Andrew Fisher, Director of Planning & Strategic Initiatives

**DATE:** November 21, 2022

**RE:** **Development Agreement – Lot 2021-2R: public hearing follow-up**

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## Introduction

At the November 28, 2022 regular meeting, Council will be asked to consider second and final reading of a development agreement for the Lot 2021-2R for a 162-unit apartment complex.

Upon request, the following provides planning staff comments with respect to the public input provided at the October Public Hearing for this application. Every written and verbal comment submitted by the public is not addressed here, but an attempt has been made to cover most if not all the major areas of concern. It is not staff's intention to invalidate the public's comments on behalf of the applicant, but rather it is to provide information that, in the opinion of planning staff, provides context or relevant information with respect to the comment.

**Walkway reserve to Donald:** When lots on the north side of Donald Avenue were subdivided around 1988, the subject 'finger' of land was reserved for a future walkway given the relatively long block of lots unbroken by a street. In theory, a walkway reserve provides for future pedestrian connectivity and reduces people 'cutting' haphazardly over private property. The walkway reserve was likely requested or required by the town planners at the time, but the property is part of the larger parcel that is subject to the apartment complex proposal – it is not town owned. Should council wish to do so, they can stipulate in the DA that the subject property can be fenced off and not used for its intended purpose. Doing so is not likely to significantly reduce pedestrian connectivity as walkers are likely to access Dickey Street via the school property or Brown Street.

**Traffic increases, pedestrian safety, speeds on Willow, too many crosswalks, too much congestion, questions about the details of the study, particularly the June 20 date of the traffic count:** The Traffic Study was completed by a licensed engineer to provide an informed estimate of the traffic that would be generated by the development and its impact on the surrounding transportation network. Given that the June 20<sup>th</sup> date of the traffic count falls within the ARHS exam schedule, it is possible that the count underrepresents pedestrian and vehicular traffic at busier times of the year. However, even if this is the case, it does not change the estimated traffic that would be generated by the development. The study estimated an Annual Average Daily Traffic Volume of 4,608 vehicles on Willow Street where an estimated 80% of the development's traffic will go. For the sake of argument, if the June 20<sup>th</sup> undercounted volumes by 50%, the resulting volume of 6,912 vehicles would still be at the low end of the Transportation Association of Canada's guidelines of 5,000 to 20,000 vehicles/day for an arterial street. There is significant pedestrian and vehicular traffic along Willow Street due in large part to students arriving and departing the two schools; however, as an arterial street this is how Willow is intended to function. In addition, higher traffic does not equal a reduced safety.

This section of Willow Street between Spring Street and Robert Angus Drive is a relatively wide street with no stop signs for almost 1.5 km. There is a misconception that a wide-open street with no obstacles is a safer street; but in reality, these circumstances tend to result in higher speeds and unexpected drivers. Perhaps counterintuitively, increased traffic, both vehicular and non-vehicular, act to slow speeds and make drivers pay closer attention to potential hazards resulting in a safer street overall. This concept can be observed in Amherst's downtown core area where street parking, pedestrians, short blocks, crosswalk bump outs and heavier traffic change driver behaviour by slowing speeds. Willow Street is not downtown Amherst, but adding another crosswalk at Gallagher will likely result in generally slower vehicle speeds in this area of Willow Street.

**Concerns about a sidewalk on the other side of Willow:** If it were ever determined to be necessary, sidewalks are typically considered a good thing for pedestrian safety. In addition, the sidewalk would be constructed within the public right-of-way.

**Increased crime:** Beyond a general per capita basis wherein more people will equal more incidences of crime, it is difficult to make a direct connection between increased crime and this proposed development.

**Large influx of people all at once:** This development when complete will result in more people in the area. The applicant has indicated that the plan is to construct Building A first and that the rate of occupancy of the first will weigh heavily on the timing of subsequent buildings. It will not be the case that all three buildings are constructed and occupied at the same time. While some residents of the subject apartments would be newcomers to Amherst, many would be existing residents moving from within the community.

**Hospital and schools not set up for this:** Capacity within the healthcare and education system is a concern to everyone, but these are provincial systems to manage and adjust to meet increased demand. Attracting more employees to work locally within these systems means addressing the currently unmet demand for housing. One way the town can help reduce the significant shortages within both the public and private work force is to address this unmet demand.

**Flooding and inadequate stormwater management:** The development agreement requires that a stormwater management plan be submitted that will include retention of stormwater during storm events. By comparison, a conventional subdivision would not include stormwater retention and would likely increase peak flow into the system.

**Dicky Brook is overgrown and prone to flooding:** The town is commencing a stormwater sewer study that will include an analysis of the capacity of Dickey Brook.

**Climate change and the dykes must be fixed:** It is difficult to draw a direct relationship between the dyke system within the Chignecto Isthmus and making a determination about this development. From a broader climate change perspective, infill higher density development within walking distance to schools and employment reduces car dependence. As such, this type of development fits the criteria of a climate-friendly form of development in several ways.

**Has the province been notified of the development?**

The Provincial Director of Planning within the Department of Municipal Affairs & Housing receives a copy of the development agreement, if approved.

**Sidewalks on Brown and Mosher Streets:** Existing and future estimated traffic volumes do not warrant the installation of sidewalks; however, the town could always decide in the future to construct new sidewalks based on actual pedestrian traffic patterns in this area.

**Negative impact on neighbourhood relationships. References to an existing supportive diverse community that will be lost with so many more people:** This is highly debatable. More people will translate into an additional diversity of backgrounds. While having more people in the area will increase the number of people that are unknown to each other, this does not necessarily translate into diminished relationships with one's neighbour.

**Overflow parking on Brown/Mosher:** It is notable that parking is permitted on all public streets. The proposed parking meets the minimum standard for apartment dwellings and there is a provision in the agreement that limits additional parking areas to the centre of the development.

**Gallagher bump out:** It is not clear what is being referred to by this comment, but as shown on the site plan provided, Gallagher will generally intersect all other streets at 90-degree angles.

**No one wants this:** Those that took the time to submit written comments and attend the two public input opportunities are opposed to the proposal. Others within that neighbourhood and beyond that might not be apposed to this development tend not to participate in public input discussions. People trying to find housing might also have different perspectives.

**Change Gallagher to a more inclusive name:** This is a good idea and easy to do as there are not any residences on Gallagher. The Street Naming Policy was recently amended to include more inclusive options.

**Freeze taxes to existing residences until construction is complete:** In practical terms, this would be impossible to implement fairly. All existing and future residences undergo construction that is intrinsically disruptive.

**Limit any development in this area to townhouses:** Townhouses have been the most prolific housing development in the last 10-20 years and tend to favour seniors over younger people that might have children. Amherst needs a mixture of housing types for people of all ages, family sizes and socioeconomic situations.

**Loss of habitat for wildlife:** A detached dwelling subdivision would result in a similar loss of habitat. When compared to large-lot, low density, string development along country highways, this higher density infill development on public services within town limits has the least direct impact on wildlife habitat.

**Proposal is too large and out of scale with the neighbourhood:** As discussed in the February and September staff reports, the proposal is clearly larger in bulk and height when compared to the single detached dwellings in the surrounding neighbourhood. Given the residential development patterns that have taken place in this area in the past, it is understandable that the subject proposal is a concern to neighbourhood residents. On the other hand, the school building immediately to the north is of a somewhat comparable footprint and overall height to the proposed buildings. The use of privacy fencing and relatively large setbacks have been employed to mitigate potential impacts on the surrounding residences.

**This is not a responsible way to grow the town:** Higher density infill development that is within walking distance of three schools is arguably a responsible way to grow the housing stock in any community. Servicing standards that include stormwater management requirements, significant separation distances, requirements to install direct access onto an arterial street and other measures, are all intended to minimize the potential negative impacts of the development. Requiring the applicant to provide shadow and traffic impact studies and other information were intended to allow the evaluation of impacts in an informed way. These measures are all elements of responsible development practices.

**What about affordable housing:** These market rental units will increase supply within a very tight local rental market, which will put downward pressure on rental rates.

**Lack of housing is resulting in unfilled jobs:** It is widely understood that the public and private sector are struggling to fill positions due to the lack of housing options.

# MEMO

**TO:** Planning Advisory Committee

**FROM:** Andrew Fisher, Director of Planning & Strategic Initiatives

**DATE:** September 6, 2022

**RE:** Development Agreement – Lot 2021-2R Brown/Mosher Apartment Complex

---

**PROPOSAL:**

An application by Paul Skerry Architects on behalf of the property owner, NovaView Developments Ltd., for a development agreement to allow construction of three, 54-unit apartment dwellings on Lot 2021-2R (PID 25002122), located south of E.B. Chandler School, adjacent to Brown and Mosher Streets.

**BACKGROUND INFORMATION:**

Committee members are referred to the February 7, 2022 staff report on this item that includes the original Application Briefing, Proposal Details, and public input from the January 26, 2022 Public Participation Opportunity. Please note that although a written submission from Mr. Giles Beland was reviewed by staff for the February staff report, it was mistakenly omitted from the February meeting information package. As such, Mr. Beland’s comments were provided to PAC members and the applicant via email and are provided separately in this package.

At the February 7, 2022 meeting, the PAC requested that the applicant provide further information and/or address the following items:

1. Shadow Analysis – request to provide a shadow analysis, particularly on Buildings A and C.
  - A shadow study is attached to this report.

Staff Comment: Unlike many larger jurisdictions, the town does not have set criteria for shade analysis. However, the shadow analysis provided does show that during the winter solstice when the sun casts the longest shadow, the adjacent dwellings will still have at least 6 hours without shade on them, which is a common criterion in other jurisdictions with established guidelines. Diagram 3 of the shadow study does show significant shade on the school building at 10 AM during Winter Solstice; however, this is unlikely to significantly impact operation of the school.

2. Perspective View –provide perspective views of the proposed buildings and existing adjacent structures.
  - Perspective views of the three buildings is attached to this report.
3. Building C – concerns were raised about the closer proximity of this building to adjacent properties along Willow Street.

- Building C and its adjacent parking lot have been flipped to reduce the mass near the Willow Street properties.

Staff Comment: changing the position of Building C increases the setback to approximately 50 m (164 ft) to the rear property line of the nearest dwelling on Willow Street.

4. Parking – the proposed parking provides 198 spaces or 1.2 spaces/unit. Concerns were raised that most units will have two vehicles. The Land Use Bylaw standard is 1.25 spaces/unit. Adding 8 spaces would satisfy the LUB 1.25/unit ratio.

- Additional parking spaces to meet the LUB 1.25 spaces/unit standard have been provided in the revised site plan.

Staff Comment: The development now meets the LUB standard and is configured such that a shortage of parking is unlikely to impact the surrounding neighborhood due to the distance to the proposed buildings. The development agreement will contain a requirement that any additional parking shall not be located closer to adjacent properties.

5. Traffic Impact Statement – request to provide a traffic impact statement by a traffic Engineer licensed to practice in N.S. that will estimate trip generation and the impact on traffic levels on the surrounding street network, including: Willow Street, Donald Avenue, Brown Street, Mosher Street, Walter Purdy Drive, and the new connection to Willow Street from the development. The statement will address the extent to which the traffic generated from the proposed development will impact the performance of streets and intersections in the vicinity in relation to the current standards for traffic volumes on local and arterial streets, as set out in the ‘TAC Manual’. If estimated trip generation exceeds surrounding capacity, estimate the number of added dwelling units that would not result in exceeding the standard.

- A Traffic Impact Study is attached to this report. Conclusions & Recommendations are listed in Section 5 on Page 39. The study estimated that the development would generate 60-64 new vehicle trips during the AM and PM peak hours, and 726 trips on a weekday. Approximately 80% of the new traffic will access Willow Street via the extension of Gallagher, with the balance using Donald Avenue via Brown/Mosher and Walter Purdy.
- The study recommended the installation of stop signs where Brown and Mosher intersect with Gallagher. It also recommended a review of the internal road network, the installation of a sidewalk along Gallaher to Willow, and a marked crosswalk on willow.
- The study also concluded that the traffic created by the proposed development ‘can be introduced safely and efficiently into the existing transportation network with the recommendations outlined’.

Staff Comment: The development agreement will require that the developer implement the recommendations from the report.

6. Landscaping Plans – request to provide more information regarding landscaping of the site and the use of vegetation and/or visual barriers to provide buffering to adjacent residential properties. Perhaps the use of opaque fencing could be used.

- A landscaping plan is attached to this report.

Staff Comment: The development agreement will require submission of a detailed Landscaping Plan, generally as shown on the Landscape Concept Plan. In particular, the developer shall erect fencing at least 6 feet in height as shown on the western side of the Landscape Concept Plan

before Building C is occupied, and shall be erected on the eastern side before Building C is occupied.

7. Propane Tanks – request to address the location of these tanks. Is there a willingness to move the tanks away from adjacent properties? If not, please provide an explanation of why this is not possible or preferred.
  - Propane tanks have been replaced with connection to natural gas.
8. Servicing Statement – confirmation from an engineer licensed in N.S. that the existing town water and sanitary sewers are of sufficient size/capacity to accommodate the development.

Staff Comment: Operations staff are confident that existing services can accommodate the development. Furthermore, the development agreement will require that the developer satisfy the Town’s Infrastructure Development Standards provided as Schedule “F” of the Town of Amherst Subdivision Bylaw. These standards apply to water, sanitary and storm services and obligate the developer to assess the impact of the development on all existing infrastructure in a manner acceptable to the Town Engineer.

9. Stormwater Management – a recent storm event resulted in significant flooding within the existing drainage ditch that runs through the subject property and extends from the northwest corner across the E.B. Chandler School property and into Dickey Brook. The proposed development will result in a significant increase in impervious surfaces and therefore the peak stormwater flows into that ditch. The Site Servicing Schematic does include some details around drainage but further analysis is required to ensure that onsite stormwater management will not exacerbate this issue.
10. Civil drawings for Building A includes a master stormwater management plan for the entire project lands. The proposed stormwater management will balance and reduce the stormwater run off generated by the project. Stormwater will be temporarily held back on site and released at a lesser rate.

Staff Comment: The Development Agreement will require that the developer’s engineer provide a design brief of the Stormwater Management Plan that explains how the plan will not result in an increase of peak flows over existing conditions

## **PUBLIC PARTICIPATION OPPORTUNITY**

A virtual public participation opportunity advertised in accordance with the Policy for Public Participation and Notification was held on January 26, 2022. A video of the meeting has been made available, and a summary is provided as part of the information package. Written submissions are also part of the information package. Some of the concerns raised at the meeting could be summarized as follows:

- Too many units and buildings are out of scale with adjacent detached dwellings.
- Traffic increase in frequency and speed
- School children safety
- Impact on school system, health care system, emergency responders, power grid, security
- Propane tank locations

- Fencing is needed
- Loss of privacy
- Construction noise and disruption
- Removal of green space and natural habitat
- Property values
- Loss of small town, established neighbourhood character
- Lack of parking

## **RELEVANT POLICY**

The following provides the Municipal Planning Strategy Policies relevant to this issue.

Land Use Bylaw: Section 7.2.2 of the Bylaw requires that residential developments with over four dwelling units are subject to a development agreement in accordance with Policy RP-9 of the Municipal Planning Strategy (MPS).

Municipal Planning Strategy:

### **Policy RP-9 Medium and High Density By Development Agreement**

*It shall be the intention of Council to ensure medium and high-density residential development occur in a manner compatible with a low-density residential neighbourhood. Specifically, Council shall require that all residential developments greater than 4 dwelling units per property, be subject to a Development Agreement. In negotiating such an agreement Council shall ensure that:*

- a) the structure is located on the lot in such a manner as to limit potential impacts on surrounding low density residential developments;*
- b) the development provides sufficient on-site parking, and appropriate access to, and egress from the street;*
- c) the location of the parking facilities does not dominate the surrounding area, including the utilization of vegetation and fences to mitigate the aesthetic impacts of parking lots;*
- d) any on site outdoor lighting does not negatively impact the surrounding properties;*
- e) any signage on the property is sympathetic to the surrounding residential properties;*
- f) vegetation is used to improve the aesthetic quality of the development;*
- g) the architecture of the building is sympathetic to any existing development in the surrounding area.*

The proposed development consists of three 4-storey buildings, which represents a significant increase in the intensity of the subject property over the single-detached dwelling lots that surround the subject property on three sides. In an effort to mitigate the impact on the neighborhood, the applicant has positioned the proposed buildings such that they have relatively large setbacks to adjacent residential properties. In response to public comment, Building C and its associated parking area were repositioned such that the setback is increased to approximately 50 m (164 ft) from the rear property line of the nearest property that fronts on Willow Street.

At the PAC's request, the developer provided a Shadow Study indicating that shadow cast will have a minimal impact on adjacent residential uses, although the adjacent school would experience shade.

Regarding parking, the proposal was revised to provide additional parking spaces to meet the LUB standard of 1.25 spaces/unit. While the developer did not provide a site plan with an optional expanded parking area(s), there is ample space within the development to expand the parking. In addition, given the relatively insular character of the development, spillover parking is unlikely to impact surrounding neighbourhood. In other words, significant street parking in the surrounding neighbourhood is unlikely given the distance to the proposed buildings.

Limiting impacts from outdoor lighting and signage can be easily dealt with under the terms and conditions of the development agreement. At the request of the PAC, the applicant provided Landscape Concept plan that includes perimeter fencing along the west and east property lines. Installation of the fencing and a detailed landscaping plan will be stipulated in the development agreement.

With regard to the architecture being sympathetic to 'any existing development in the surrounding area', the proposed buildings are clearly larger in both bulk and height in comparison to the adjacent detached dwellings. On the other hand, each building is approximately half the footprint and roughly 20 feet greater in height in comparison to E.B Chandler School located immediately to the north of the property. Further afield, there are existing three storey apartment buildings along Dickey Street to the west and Spring Street to the north.

### **GP-7 Compatibility**

*It shall be the intention of Council to allow a mix of compatible land uses to minimize their impacts by:*

- (a) requiring adequate buffering and setbacks;*
- (b) screening development by the use of visual barriers; and,*
- (c) regulating the location of parking, storage buildings and other accessory uses or facilities.*

The matters noted in GP-7 are for the most part included in the discussion above regarding RP-9. In addition, the location of accessory features such as storage facilities for solid waste can be addressed by the applicant and provided for in the development agreement.

### **GP-8 Density**

*It shall be the intention of Council to allow development at a density appropriate to the overall desired character of the town.*

While the relatively high density of the proposed development is a concern, particularly to many residents in the nearby area, the proposal does represent a mix of housing type and densities, which corresponds to the general intent other MPS policies noted in this report.

### **RP-8 Housing Mix**

*It shall be the intention of Council to encourage a mix of housing densities in all residential areas of town to encourage a mix of housing types and income groups in all residential areas.*

The proposal would significantly increase the overall housing density in the area.

### **RP-10 Neighbourhood Stabilization**

*It shall be the intention of Council to provide for the stabilization of existing residential neighbourhoods by: (b) discouraging the encroachment of non-compatible land uses.*

The application is for a multi-unit residential development surrounded by other residential uses and an institutional use. When compared to an industrial use, it could be argued that the existing and proposed uses are all compatible. Where this policy may be relevant is the extent to which the proposed development is so intense as to be non-compatible with its surroundings. Mitigating factors include the building design and location, site layout, the use of vegetative or fencing buffers, as well as, an analysis of the impact of the proposal on shadow casting and the transportation network. These factors have all been addressed to some degree by the applicant.

### **RP-11 Affordable Housing**

*It shall be the intention of Council to encourage and promote the provision of affordable housing units within all residential areas of the Town by:(a) encouraging a mix of housing types and densities;*

The proposal would significantly contribute to the ‘mix of housing types and increase the overall density in the area. Currently, the shortage of housing across the spectrum of housing types has had a significant negative impact on housing affordability. Additions of these multi-unit dwellings addresses this shortage and can potentially increase affordability.

### **RP-12 Residential Area Design**

*It shall be the intention of Council to ensure that new residential areas:*

- a) provide for the efficient use of land;*
- b) provide for the efficient and economic extension of existing water, storm sewer and sanitary sewer systems and other utilities;*
- c) incorporates a hierarchy of streets that efficiently and safely accommodates traffic flows and proper access to other areas of Town;*
- d) provides for the efficient and safe movement of pedestrians and cyclists;*
- e) minimizes adverse effects on the environment;*
- f) provides for parks and other community uses in safe and central locations.*

The proposal is an infill development, which speaks to the efficient use of land that does not require extension of existing town infrastructure. Vehicle and active transportation connectivity is relatively high, with two existing street accesses via Brown and Mosher Streets to Walter Purdy Drive. The most important connection will be direct access to Willow Street via a newly constructed public access. The development is within close walking distance to three schools, providing easy pedestrian access for children, and it is in close proximity to the trail that runs along Dickey Brook that connects to parks to the west and the marsh trail to the northwest. Dickey Park is within short walking distance.

As noted previously, the Traffic Impact Study found that the estimated new vehicle and pedestrian traffic can be ‘safely and efficiently’ introduced into the existing transportation network with recommended actions that include a marked crosswalk on Willow Street.

#### **MS-4 Service Standards**

*It shall be the intention of Council to maintain a ‘Development Standards Bylaw’ in order to establish the required servicing standards for development within the town...*

The applicant has been provided with the town’s Development Standards, which are part of the Subdivision Bylaw. Town engineering have indicated that existing service connections can accommodate the proposal. Preliminary servicing and storm drainage plan have been submitted. A standard development agreement would require the submission of a storm water drainage plan as part of the development. Principal requirements of the drainage plan will be not increase runoff onto adjacent properties, and not increase peak flows into Dickey Brook through the installation of retention infrastructure.

#### **MS-11 In-fill Development**

*It shall be the intention of Council to encourage and facilitate the development of vacant land located on existing municipal services in order to make more efficient use of such services.*

As previously stated, the proposal satisfies the above policy.

#### **R-21 High Density Open Space**

*It shall be the intention of Council to require multiple unit residential properties to provide usable open space for use of residents on the site.*

In addition to the individual balconies, the proposal provides significant common open space, including over an acre in the center of the development.

#### **A-5: Amendment Criteria**

*It shall be the intention of Council, when considering [...] entering into a development agreement, to consider the following matters, in addition to all other criteria set out in the various policies of this planning strategy:*

- (a) That the proposal conforms to the general intent of this plan and all other municipal bylaws and regulations.*
- (b) That the proposal is not premature or inappropriate by reason of:
  - (i) the financial capability of the Town to absorb any costs relating to the development;*
  - (ii) the adequacy of municipal water, sanitary sewer and storm sewer services;*
  - (iii) the adequacy of road networks, in, adjacent to, or leading to the development;**
- (c) That consideration is given to the extent to which the proposed type of development might conflict with any adjacent or nearby land uses by reason of:
  - (i) type of use;*
  - (ii) height, bulk and lot coverage of any proposed building;*
  - (iii) parking, traffic generation, access to and egress from the site;*
  - (iv) any other matter of planning concern outlined in this strategy.**

While it could be argued that the proposal generally conforms to the general intent of the MPS policies, there is legitimate cause for concern with regard to traffic generation and the extent to which the number of dwelling units and the height, bulk of the buildings may impact the surrounding neighbourhood. The applicant has attempted to address these concerns by providing a Shadow Study, changed building and parking locations to increase separation, and proposed privacy fencing to mitigate the impact of the parking lots on adjacent residential uses to the east and west.

A traffic impact study conducted by a traffic engineer licensed to practice in Nova Scotia was conducted to provide an analysis of the capacity impact of the surrounding street network as a result of the estimated traffic generated from the development. As outlined previously, the study concluded that the new traffic generated from the development could be accommodated into the existing transportation network.

### **CONCLUSIONS:**

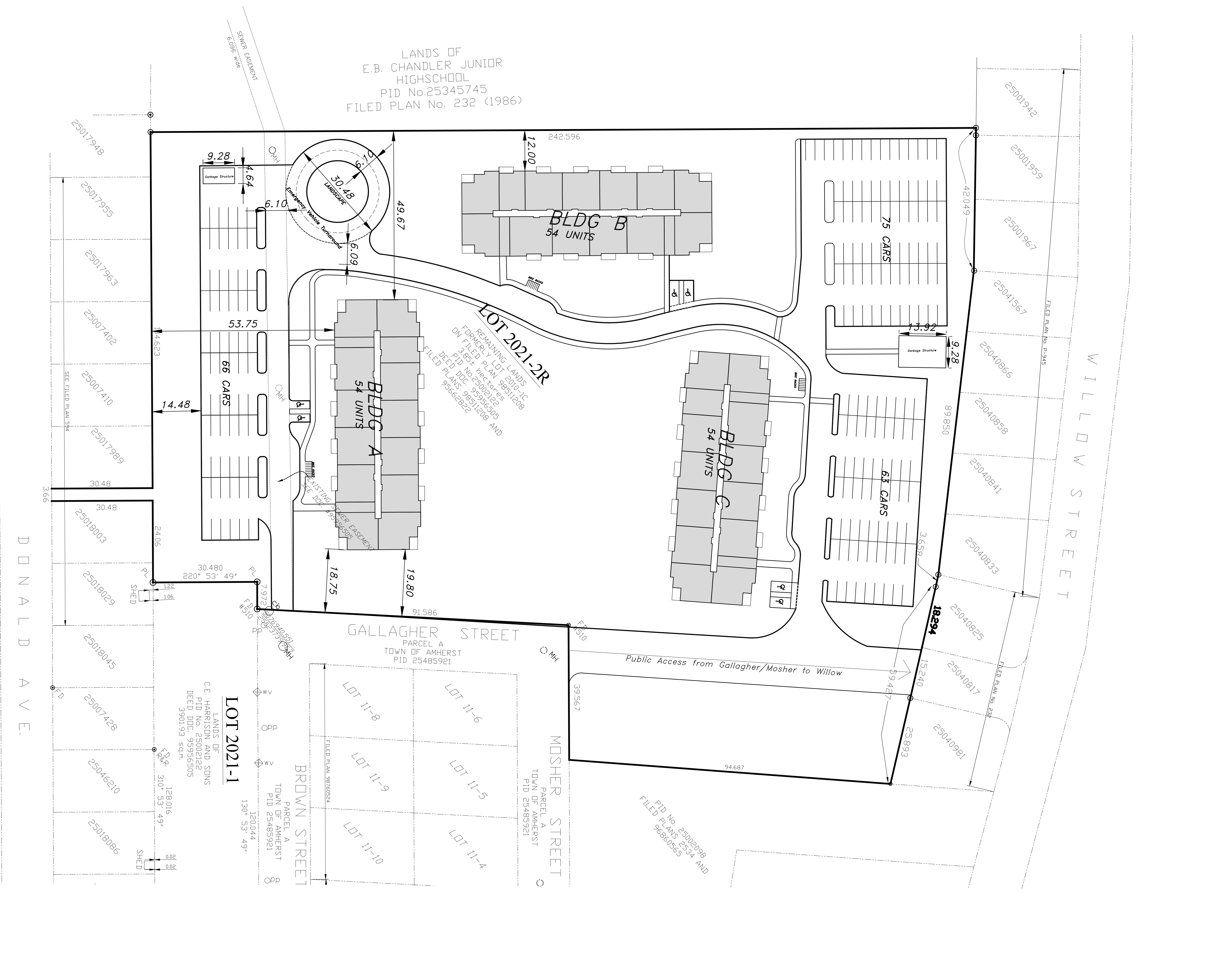
As discussed above, the proposal meets the general intent of MPS policies. The development agreement process has given the public an opportunity to provide input that is detailed in this package. Should Council decide to give First Reading of the agreement, the public will have another opportunity to provide input at a public hearing before Council. The PAC, after reviewing the material, requested additional information and changes that the applicant has for the most part provided. With regard to addressing and/or mitigating the potential negative impacts, reasonable attempts have been made to address these issues and have been incorporated into the draft development agreement.

Staff recognize the scale of the proposed development is cause for concern; however, there is no specific MPS policy that would prohibit or significantly reduce the proposal's scale. Policies that relate to traffic impacts, separation distances, parking provisions, use of fencing and landscaping, shadow casting, engineering analysis for servicing, and other measures have all been addressed. When compared to other urban centres, it is difficult to argue that 4-storey apartment buildings cannot be compatible with single-detached dwellings.

### **OPTIONS:**

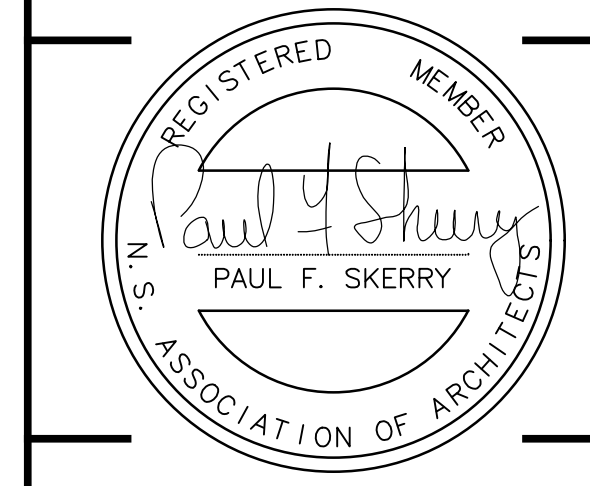
- Option One: Recommend that Council enter into the Development Agreement for Lot 21-2R as drafted.
- Option Two: Recommend that Council enter into the Development Agreement for Lot 21-2R with specific changes to the draft agreement as follows:
- Option Three: Recommend to Council not to enter into the Development Agreement for Lot 21-2R, siting specific policies with which the proposal does not conform.

**STAFF RECOMMENDATION: Option One.**



- NOTES:
- 1) THE CONTRACTOR IS RESPONSIBLE FOR CHECKING ALL DIMENSIONS ON SITE & REPORTING ANY DISCREPANCY TO THE ARCHITECT BEFORE PROCEEDING.
  - 2) DO NOT SCALE FROM DRAWINGS. USE FIGURED DIMENSIONS.
  - 3) DRAWING REPRESENTATIONS MAYBE IN VARIANCE W/ DETAILED SPECS. & SCHEDULES, IN WHICH CASE SPECS. & SCHEDULES OVERRIDE THE DRAWINGS.
  - 4) CHANGES FROM THESE PLANS & SPECS. MUST BE AGED TO IN WRITING, & APPROVED BY THE ARCHITECT & OWNER, BEFORE PROCEEDING.
  - 5) ONLY THOSE DRAWINGS MARKED APPROVED FOR CONSTRUCTION, SIGNED & DATED BY THE ARCHITECT ARE TO BE USED FOR CONSTRUCTION.
  - 6) THESE DRAWINGS ARE TO BE READ IN CONJUNCTION W/ THE SPECS.

Paul Skerry Associates Ltd.  
**ARCHITECTS**  
5514 LIVINGSTONE PLACE  
HALIFAX N.S. B3K 2B9  
PHONE: 902-455-4361  
FAX: 902-455-7778  
EMAIL: pskerry@ns.symptotico.ca



No.	DESCRIPTION	Date
09	Repositioned BLD C	4/28/22
08	Removed Propane Tanks	4/28/22
07	Added Propane Tanks	10/29/21
06	Increased Turing Circle Size	10/13/21
05	Added Garbage Structures	10/09/21
04	Revised Site Plan per Civil	10/09/21
03	Revised Site Plan	9/21/21
02	Revised Public Access Road	4/29/21
01	Revised Site Plan	4/22/21

REVISIONS

PROJECT

PROPOSED PROJECT  
Lot 2021-2R  
BROWN/GALLAGHER/MOSHER  
STREETS  
AMHERST, NS.

TITLE:  
SITE PLAN

drawn by: CM	checked: PS
date: April 26/2022	approved: PS
scale: 1:500	dwg #: A1.0
project #: 3128	





## Andrew Fisher

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**From:** Kim Jones  
**Sent:** January 21, 2022 1:49 PM  
**To:** Andrew Fisher  
**Subject:** FW: Questions for Dev. Agreement Application - Multi-Unit Brown/Mosher Streets  
**Attachments:** Side Set Elevation - back 97 willow.pdf

FYI

**From:** Giles Beland - Bowers' Construction <[REDACTED]@gmail.com>  
**Sent:** January 21, 2022 10:52 AM  
**To:** Kim Jones <KJones@amherst.ca>  
**Subject:** Questions for Dev. Agreement Application - Multi-Unit Brown/Mosher Streets

Good morning.

The following are some of my questions for the meeting scheduled on Jan 26, 2022.

- What is the developer's history? If it is a new partnership, what are the individual principles development histories?
- What other projects have the developer built & where?
- How have the neighbourhoods where this developer built reacted to their projects?
- Who is the target market for these residential apartments?
- What will be the monthly rental rates for these apartments - range, please.
- Will there be inclusionary housing units provided within these apartments? If so, how many or percentage?
  
- Bldg's A & B enjoy a parking lot buffer between their structures and the property boundaries, but Bldg C does not - this is **not** acceptable.  
BUILDING C should be located where the proposed parking lot is located and the parking lot where Bldg C is currently being offered.  
I have attached a side elevation of the shadow cast by the proposed building location vs where I suggest it is relocated. (40' high apartment block, 14' high residence)
  
- Is there a plan or time frame for construction? Start date? Which structure will be built first? Or, will they all be built at the same time?
- What privacy designs are being proposed between property owners and the new development? Fence? Style, height, etc.
- Will there be pedestrian access along the three sides not accessed by a road? EB Chandler, for example.
- What is the ground lighting plan? What consideration of light pollution has been given to existing property residents?
- What is the plan for crosswalk provisions at the intersection of Gallagher & Willow?
- Will there be a sidewalk along Gallagher street? If so, what side?
- What will be the finish grade elevation of the property concerning the existing elevation?
- Are the current municipal services, water, sewer & storm adequate for this proposal? If not, what are the expected costs to enhance to meet the requirements? Who will pay for these enhancements? The developer or the town? Where will the money come from?
- How will garbage be collected? Will there be dumpsters? If so, where will they be located? With what frequency will they be serviced, and at what time of the day?
- How will recycling (as currently participating residents follow) be collected? Where will these recyclables be located? With what frequency will they be serviced, and at what time of the day?

- Where will the snow be stockpiled? What are the plans to minimize runoff into adjacent properties?
- What are the proposed tree planting requirements? Where will trees be planted? What kind of trees will be planted?

Will all these questions be asked at this meeting? If not, why? When will they be answered?

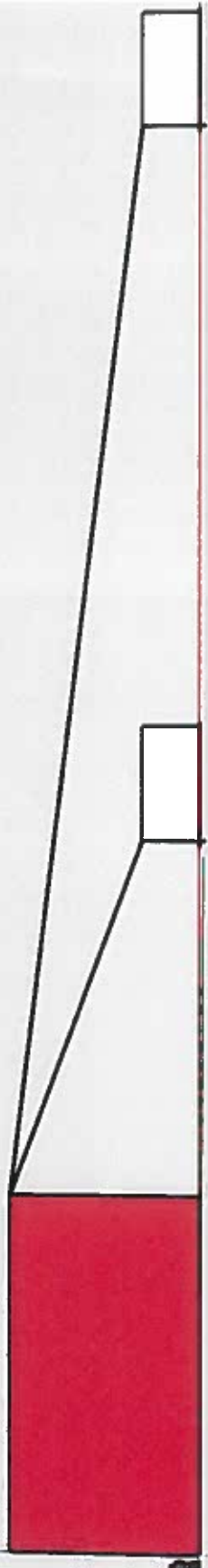
Regards,  
Giles Beland  
97 Willow Street  
Amherst NS

--  
Giles Beland, M.Pl.  
Bowers' Construction Ltd.  
25 Lorne Street  
Sackville, NB E4L 3Z8

  
[www.bowersconstruction.com](http://www.bowersconstruction.com)

*"a jack of all trades is a master of none, but oftentimes better than a master of one"*

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# MEMO

**TO:** Planning Advisory Committee

**FROM:** Andrew Fisher, Director of Planning & Strategic Initiatives

**DATE:** February 7, 2022

**RE:** **Development Agreement – Lot 2021-2R Brown/Mosher Apartment Complex**

---

## **PROPOSAL:**

An application by Paul Skerry Architects on behalf of the property owner, Nova View Developments Ltd., for a development agreement to allow construction of three, 54-unit apartment dwellings on Lot 2021-2R (PID 25002122), located south of E.B. Chandler School, adjacent to Brown and Mosher Streets.

## **BACKGROUND INFORMATION:**

The attached Application Briefing provides the Site Details, Neighbourhood Context, and Proposal Details.

## **PUBLIC PARTICIPATION OPPORTUNITY**

A virtual public participation opportunity advertised in accordance with the Policy for Public Participation and Notification was held on January 26, 2022. A video of the meeting has been made available, and a summary is provided as part of the information package. Written submissions are also part of the information package. Some of the concerns raised at the meeting could be summarized as follows:

- Too many units and buildings are out of scale with adjacent detached dwellings.
- Traffic increase in frequency and speed
- School children safety
- Impact on school system, health care system, emergency responders, power grid, security
- Propane tank locations
- Fencing is needed
- Loss of privacy
- Construction noise and disruption
- Removal of green space and natural habitat
- Property values
- Loss of small town, established neighbourhood character
- Lack of parking

## **RELEVANT POLICY**

The following provides the relevant Municipal Planning Strategy Policies

Land Use Bylaw: Section 7.2.2 of the Bylaw requires that residential developments with over four dwelling units are subject to a development agreement in accordance with Policy RP-9 of the Municipal Planning Strategy (MPS).

Municipal Planning Strategy:

**Policy RP-9 Medium and High Density By Development Agreement**

*It shall be the intention of Council to ensure medium and high-density residential development occur in a manner compatible with a low-density residential neighbourhood. Specifically, Council shall require that all residential developments greater than 4 dwelling units per property, be subject to a Development Agreement. In negotiating such an agreement Council shall ensure that:*

- a) the structure is located on the lot in such a manner as to limit potential impacts on surrounding low density residential developments;*
- b) the development provides sufficient on-site parking, and appropriate access to, and egress from the street;*
- c) the location of the parking facilities does not dominate the surrounding area, including the utilization of vegetation and fences to mitigate the aesthetic impacts of parking lots;*
- d) any on site outdoor lighting does not negatively impact the surrounding properties;*
- e) any signage on the property is sympathetic to the surrounding residential properties;*
- f) vegetation is used to improve the aesthetic quality of the development;*
- g) the architecture of the building is sympathetic to any existing development in the surrounding area.*

The proposed development consists of three 4-storey buildings, which represents a significant increase in the intensity of the subject property over the single-detached dwelling lots surround the subject property on three sides. In an effort to mitigate the impact on the neighborhood, the applicant has positioned the proposed buildings such that they have relatively large setbacks to adjacent residential properties. One way to evaluate the degree to which the setbacks are adequate to limit potential impacts is to conduct a shadow study, which will show how adjacent properties may or may not be shaded from the sun by the proposed buildings. While the intention of the shade study would not be to eliminate any shade cast to adjacent properties, it could result in adjustments to the proposal to reduce potential impacts.

The proposal includes 1.2 parking spaces per unit totaling approximately 200 spaces. The lots spread over three areas to mitigate against dominating the property. Given the somewhat insular configuration of the proposal it is unlikely that a shortage of parking spaces would impact adjacent properties; however, the agreement could require that the developer provide a site plan showing where additional parking spaces could be provided onsite should they be found necessary once all three buildings are occupied. In addition, it may be prudent to move the 56-space parking area such that it is located between Building C and the adjacent properties that front on Willow Street. The shadow study noted above could inform that decision.

Limiting impacts from outdoor lighting and signage can be easily dealt with under the terms and conditions of the development agreement. With regard to the use of vegetation, it is reasonable to request that the proponent provide detail around how the use of vegetation and or fencing might be used to improve the aesthetic quality of the development, as well as, mitigate the potential

impacts on adjacent properties. It is also reasonable to recommend that Council enter into a development agreement, subject to the submission of a detailed landscaping plan.

With regard to the architecture being sympathetic to ‘any existing development in the surrounding area’, the proposed buildings are clearly larger in both bulk and height in comparison to the adjacent detached dwellings. On the other hand, each building is approximately half the footprint and roughly 20 feet greater in height in comparison to E.B Chandler School located immediately to the north of the property. Further afield, there are existing of three storey apartment buildings along Dickey Street to the west and Spring Street to the north.

### **GP-7 Compatibility**

*It shall be the intention of Council to allow a mix of compatible land uses to minimize their impacts by:*

- (a) requiring adequate buffering and setbacks;*
- (b) screening development by the use of visual barriers; and,*
- (c) regulating the location of parking, storage buildings and other accessory uses or facilities.*

The matters noted in GP-7 are for the most part included in the discussion above regarding GP-9. In addition, the location of accessory features such as storage facilities for propane and solid waste can be addressed by the applicant and provided for in the development agreement.

### **GP-8 Density**

*It shall be the intention of Council to allow development at a density appropriate to the overall desired character of the town.*

While the relatively high density of the proposed development is a concern, particularly to many residents in the nearby area, the proposal does represent a mix of housing type and densities, which corresponds to the general intent other MPS policies noted in this report.

### **RP-8 Housing Mix**

*It shall be the intention of Council to encourage a mix of housing densities in all residential areas of town to encourage a mix of housing types and income groups in all residential areas.*

The proposal would significantly increase the overall housing density in the area.

### **RP-10 Neighbourhood Stabilization**

*It shall be the intention of Council to provide for the stabilization of existing residential neighbourhoods by: (b) discouraging the encroachment of non-compatible land uses.*

The application is for a multi-unit residential development surrounded by other residential uses and an institutional use. As such, all such uses are intrinsically compatible. Where this policy may be relevant is the extent to which the proposed development is so intense as to be non-compatible with its surroundings. Mitigating factors include the building design and location, site layout, the use of vegetative or fencing buffers, as well as, an analysis of the impact of the proposal on shadow casting and the transportation network.

### **RP-11 Affordable Housing**

*It shall be the intention of Council to encourage and promote the provision of affordable housing units within all residential areas of the Town by:(a) encouraging a mix of housing types and densities;*

The proposal would significantly contribute to the ‘mix of housing types and increase the overall density in the area.

### **RP-12 Residential Area Design**

*It shall be the intention of Council to ensure that new residential areas:*

- a) provide for the efficient use of land;*
- b) provide for the efficient and economic extension of existing water, storm sewer and sanitary sewer systems and other utilities;*
- c) incorporates a hierarchy of streets that efficiently and safely accommodates traffic flows and proper access to other areas of Town;*
- d) provides for the efficient and safe movement of pedestrians and cyclists;*
- e) minimizes adverse effects on the environment;*
- f) provides for parks and other community uses in safe and central locations.*

The proposal is an infill development, which speaks to the efficient use of land that does not require extension of existing town infrastructure. Vehicle and active transportation connectivity is relatively high, with two existing street accesses via Brown and Mosher Streets to Walter Purdy Drive. The most important connection will be direct access to Willow Street via a newly constructed public access. The development is within close walking distance to three schools, providing easy pedestrian access for children, and it is in close proximity to the trail that runs along Dickey Brook that connects to parks to the west and the marsh trail to the northwest. Dickey Park is within short walking distance.

### **MS-4 Service Standards**

*It shall be the intention of Council to maintain a ‘Development Standards Bylaw’ in order to establish the required servicing standards for development within the town...*

The applicant has been provided with the town’s Development Standards, which are part of the Subdivision Bylaw. Town engineering have indicated that existing service connections can accommodate the proposal. Preliminary servicing and storm drainage plan have been submitted. A standard development agreement would require the submission of a storm water drainage plan as part of the development. Principal requirements of the drainage plan will be not increase runoff onto adjacent properties, and not increase peak flows into Dickey Brook through the installation of retention infrastructure.

### **MS-11 In-fill Development**

*It shall be the intention of Council to encourage and facilitate the development of vacant land located on existing municipal services in order to make more efficient use of such services.*

As previously stated, the proposal satisfies the above policy.

### **R-21 High Density Open Space**

*It shall be the intention of Council to require multiple unit residential properties to provide usable open space for use of residents on the site.*

In addition to the individual balconies, the proposal provides significant common open space, including over an acre in the center of the development.

### **A-5: Amendment Criteria**

*It shall be the intention of Council, when considering [...] entering into a development agreement, to consider the following matters, in addition to all other criteria set out in the various policies of this planning strategy:*

- (a) That the proposal conforms to the general intent of this plan and all other municipal bylaws and regulations.*
- (b) That the proposal is not premature or inappropriate by reason of:
  - (i) the financial capability of the Town to absorb any costs relating to the development;*
  - (ii) the adequacy of municipal water, sanitary sewer and storm sewer services;*
  - (iii) the adequacy of road networks, in, adjacent to, or leading to the development;**
- (c) That consideration is given to the extent to which the proposed type of development might conflict with any adjacent or nearby land uses by reason of:
  - (i) type of use;*
  - (ii) height, bulk and lot coverage of any proposed building;*
  - (iii) parking, traffic generation, access to and egress from the site;*
  - (iv) any other matter of planning concern outlined in this strategy.**

While it could be argued that the proposal generally conforms to the general intent of the MPS policies, there is legitimate cause for concern with regard to traffic generation and the extent to which the number of dwelling units and the height, bulk of the buildings may impact the surrounding neighbourhood. As noted above in the RP-9 discussion, a shadow cast analysis would show the degree to which the proposed buildings would shadow adjacent properties, if at all. This information would help inform the review of the proposal.

With regard to traffic generation, there is little doubt the development will increase traffic in the area. A traffic impact study conducted by a traffic engineer licensed to practice in Nova Scotia would provide an analysis of the capacity impact of the surrounding street network as a result of the estimated traffic generated from the development. The study would answer whether or not the combined traffic generated from the existing and proposed development will be within the current capacity standards as established by the Transportation Association of Canada. The study scope would include Willow, Donald, Brown, Mosher, and Walter Purdy Drive.

**CONCLUSION:**

As discussed above, the proposal meets the general intent of town policies, but more information would help making an informed a decision on the application. As per the development agreement process, the public have been given an opportunity to provide input. It would be prudent for the PAC to give their feedback to the applicant, request more information in the form of a shadow and traffic impact analysis, and give the applicant an opportunity to respond.

**OPTIONS:**

Option One: Defer a recommendation to Council and request that the applicant provide a shadow study of the proposed buildings, a traffic impact analysis to the satisfaction of the Development Officer, and attempt to address concerns raised by the public and the PAC.

Option Two: Recommend to Council not to enter into the Development Agreement for Lot 21-2R, siting specific policies with which the proposal does not conform.

**STAFF RECOMMENDATION: Option One.**

# Application Details

**Applicant:** Connor Manson, Paul Skerry Architects Ltd.

**Owner:** Nova View Developments Ltd.

**Summary of Proposal:** Three, 54-unit Apartment Buildings.

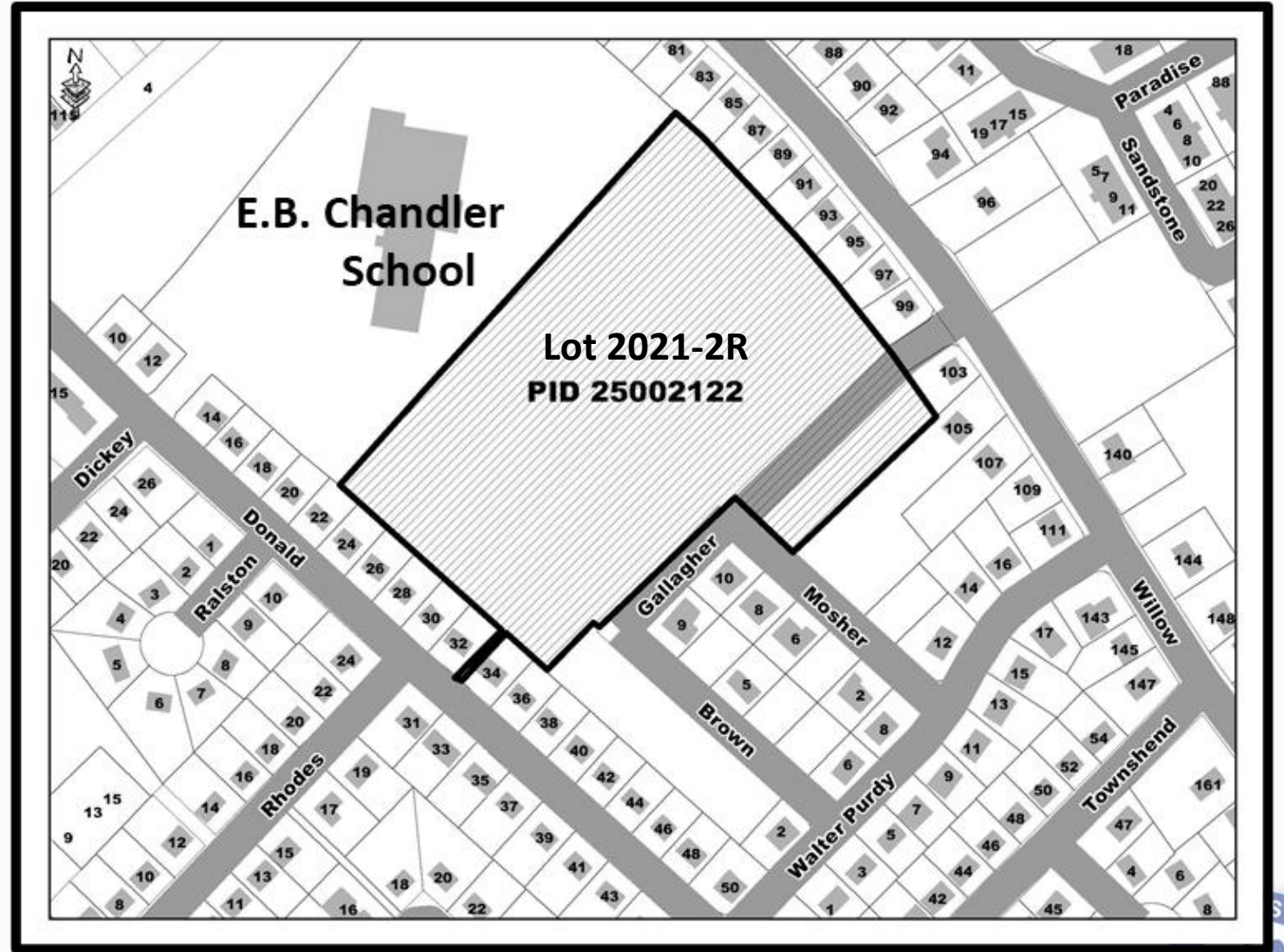
**Location:** Lot 2021-2R south of E.B. Chandler School, adjacent to Brown and Mosher Streets.

**Property Size:** 38,500 m<sup>2</sup> (9.5 acre)

**Street Frontage:** 91.5 m at Gallagher, 15 m at Willow.

**Existing Land Use:** vacant field.

**Existing Zoning:** General Residential



# Neighbourhood Context

- The Subject property is immediately surrounded on three sides by single-detached dwellings.
- Several existing multi-unit developments exist within the general area, including but not limited to:
  - Two 24-unit buildings on Dickey St.
  - 12-unit apartment on Willow St.
  - 33-unit apartment on Spring St.
  - Several 4-unit townhouses to the east in the Sandstone Crescent area and 4-6 unit townhouses to the west in the Dickey/Rupert Street area.
- E.B. Chandler Junior Highschool is immediately adjacent to the north. Spring Street Academy and Amherst Regional High School is approximately 500 metres away.
- Centennial Trail, and Dickey Park are some of the recreational spaces within short walking distance of the subject property.



# Proposal Details

**General Description:** Construct three, 54-unit four storey apartment buildings of identical size, architectural design, and features.

**Number of Units:** 54 / building x 3 = 162 total

**Property Area:** 38,500 m<sup>2</sup> (9.51 acre)

**Density:** 17 units/acre

**Parking:** 198 spaces on 3 lots (1.2 spaces/unit)

**Access:** vehicle access from Brown and Mosher Streets, and connection of Gallagher Street to Willow Street. Pedestrian access to Donald Ave.

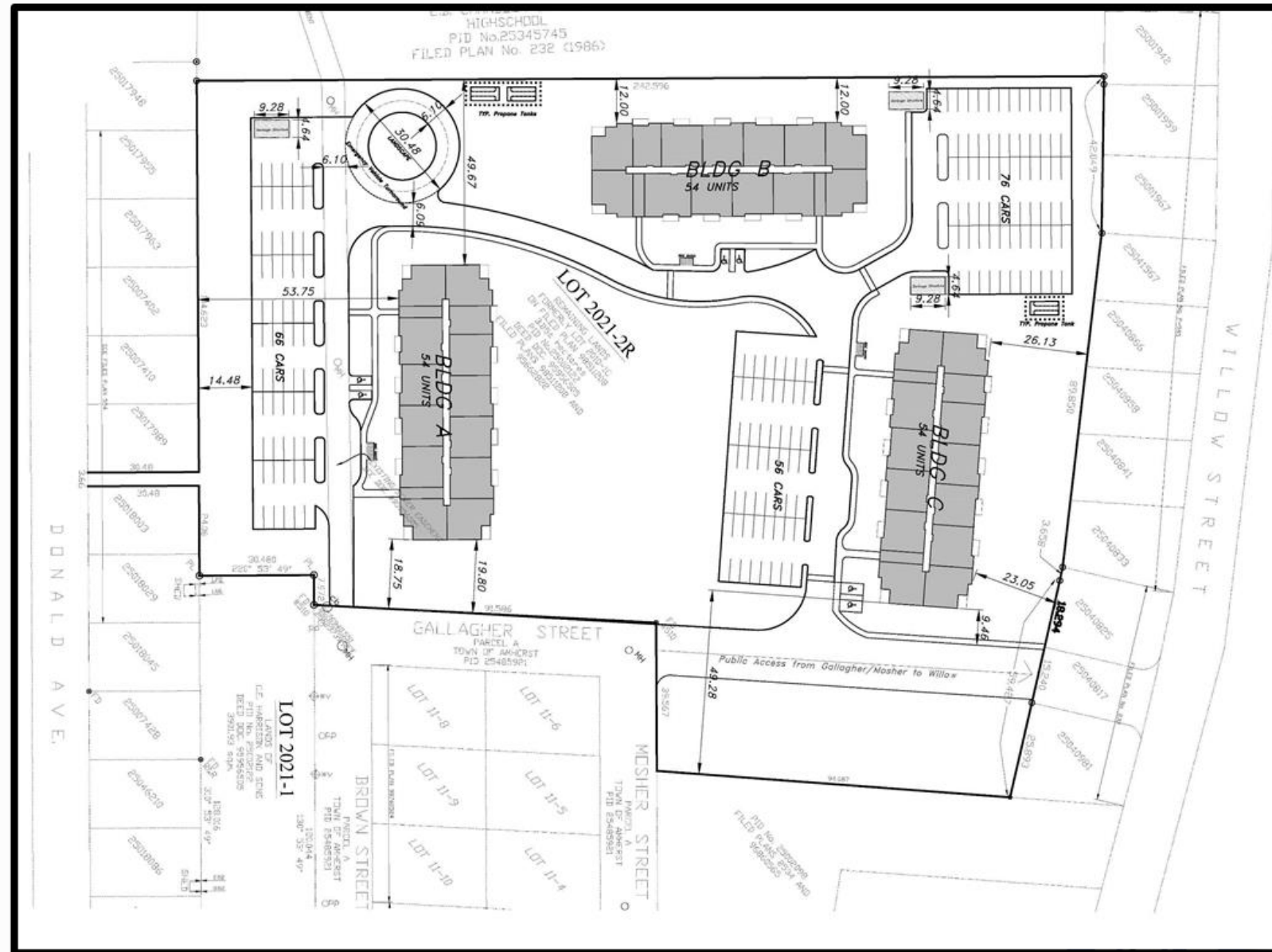
**Building Dimensions:** 76 feet x 240 feet

## Setbacks:

**Building A** – 54 metres (176 ft.) to lots fronting Donald Ave. 34 metres (112 ft.) to nearest lot on Brown Ave.

**Building B** – 12 metres (39 ft.) to EB Chandler School lot.

**Building C** – approx. 25 metres (80 ft.) to lots fronting on Willow Street.



# Building Details

**Building Dimensions:** 23.2 m (76 ft.)  
x 73.2 m (240 ft.)



3D View 1



3D View 2

**PRELIMINARY  
NOT FOR CONSTRUCTION**

# Building Details

**Building Dimensions:** 23.2 m (76 ft.) x 73.2 m (240 ft.)

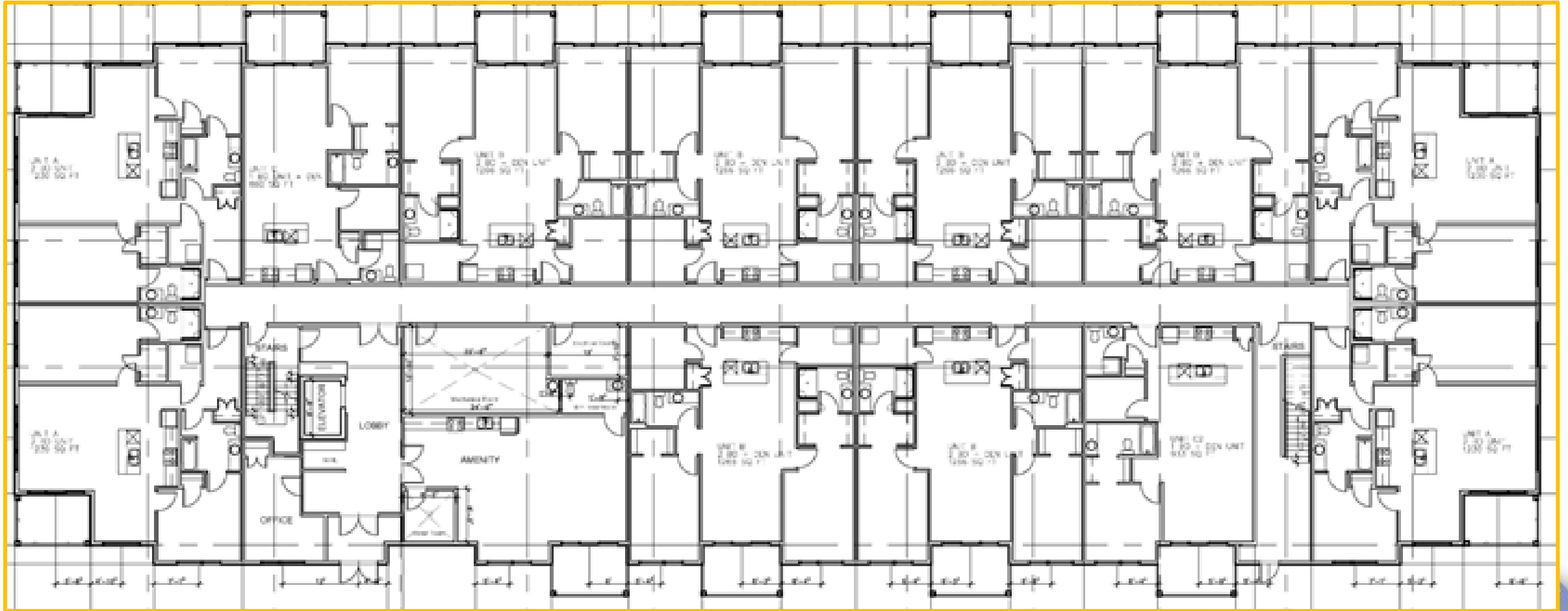


**PRELIMINARY  
NOT FOR CONSTRUCTION**

# Ground Floor

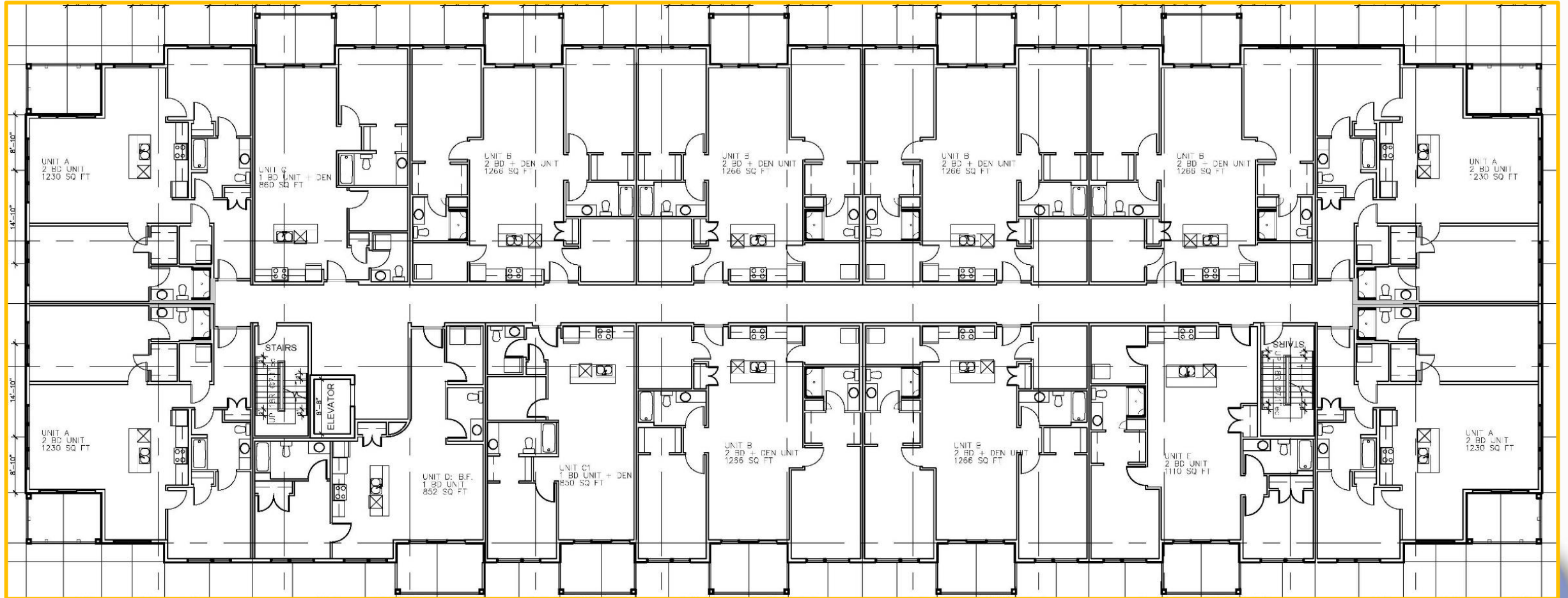
- Ten 2 bedroom units (1,230 – 1,266 sqft)
- Two 1 bedroom units (850 – 860 sqft)
- All units include a den and 96 sqft outdoor patio

Common Amenity Space (approx. 500 sqft)



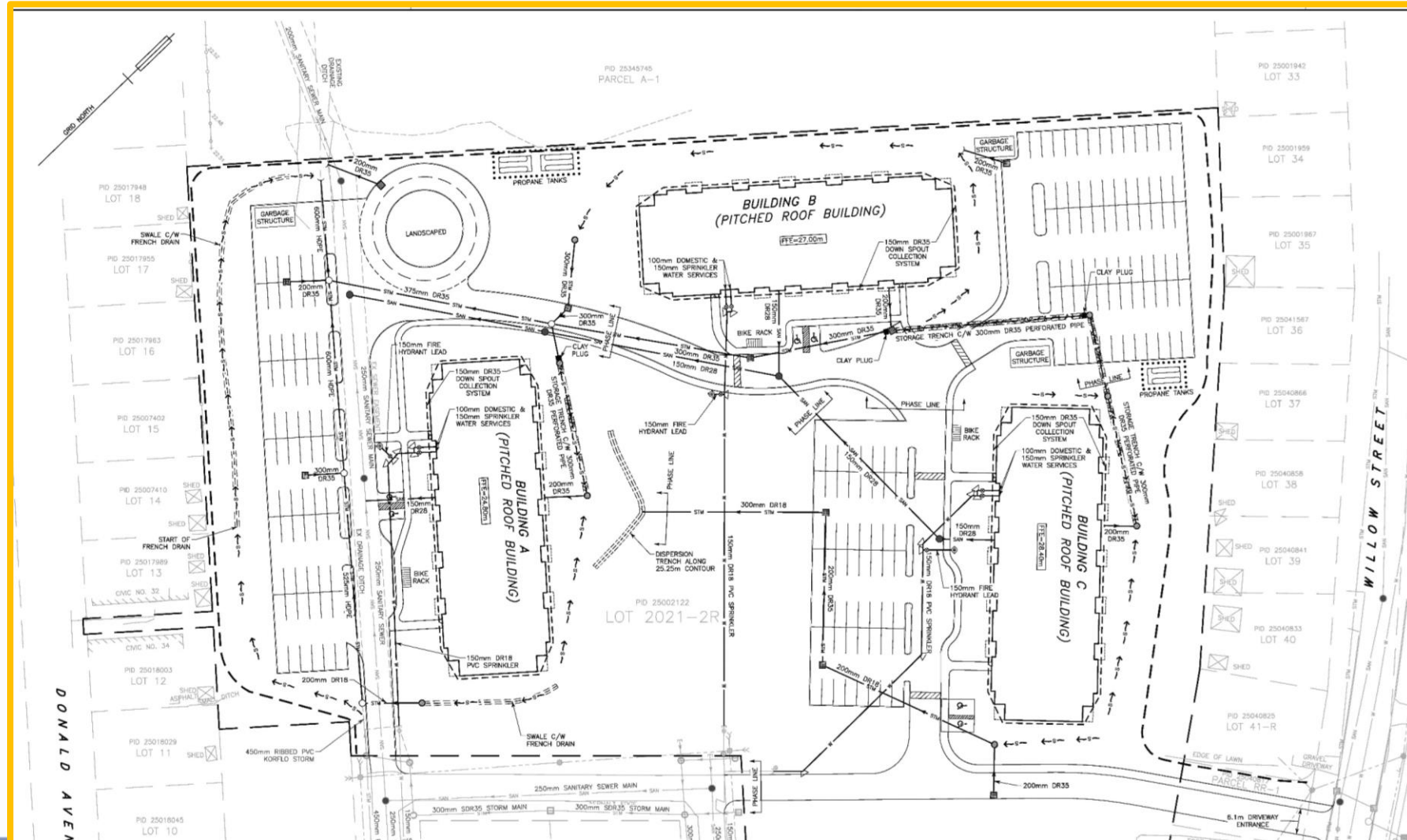
# Floors 2-4

- Eleven 2 bedroom units (1,230 – 1,266 sqft)
- Three 1 bedroom units (860 – 933 sqft)
- All units have a den and a 96 sqft outdoor patio



# Site Servicing

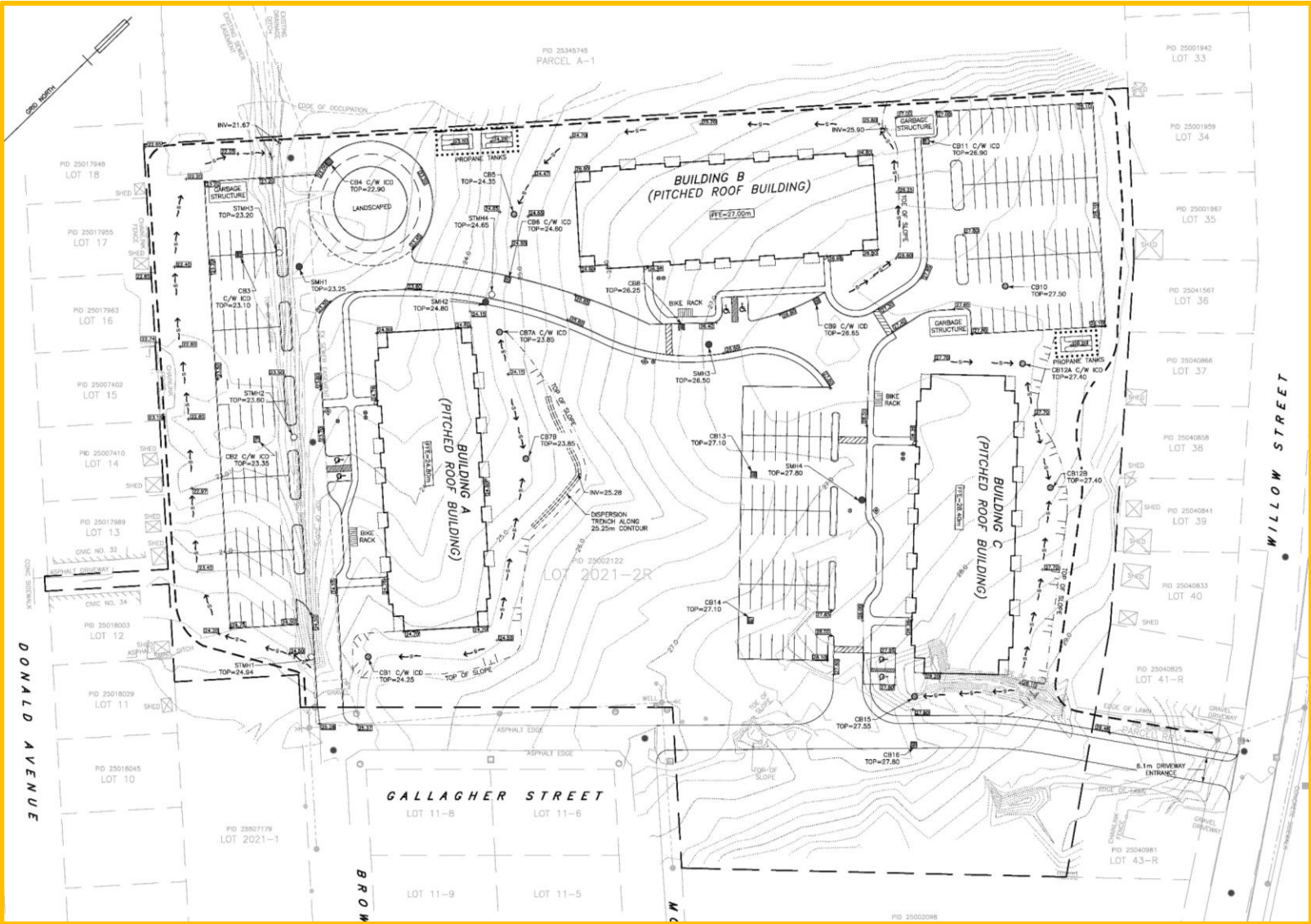
- Preliminary design developed in coordination with town engineering based on Town Servicing Standards
- Utilizes existing water and sanitary service mains



#seewhyweloveit



# Surface Drainage



**From:** Jeff Smith <[jeff@jeffsmith.me](mailto:jeff@jeffsmith.me)>  
**Sent:** January 14, 2022 12:20 PM  
**To:** Kim Jones <[KJones@amherst.ca](mailto:KJones@amherst.ca)>  
**Cc:** Andrew Fisher <[AFisher@amherst.ca](mailto:AFisher@amherst.ca)>  
**Subject:** Feedback on Lot 2021-2R (PID 25002122) Brown/Mosher Streets  
**Importance:** High

Good afternoon,

I reside at 88 Willow Street and this morning received notification of the proposed plans for Lot 2021-2R Brown/Mosher Streets.

I must state that I am honestly appalled that Amherst Town Council would consider rezoning this land and supporting this project. Willow Street already has a very high volume of traffic without adding the possibility of another 198 vehicles to traffic in the area as well as the heavy equipment and increased construction traffic during development. This will create a very dangerous situation for children that live in the area as well as all children that are walking to Amherst Regional High School, EB Chandler Junior High, and Spring Street Academy. There are already close calls with traffic and kids crossing Willow Street due to the high rate of speed of many vehicles, especially during higher traffic times in the morning, noon time, and after school and work. Given that I have three young children myself, the thought of potentially doubling the traffic in this area is not only terrifying as a parent, but as a homeowner in the area the significant noise, traffic, and disruption that will be added will make this area of town very undesirable to live.

I am already hesitant to allow my children to play in our front yard due to the volume of traffic that is already present on Willow Street.

In addition to these *significant* safety concerns, the proposed buildings are entirely out of character and scale with existing architecture in the area. These buildings will be towering over surrounding houses, reducing the amount of natural light these properties receive. Privacy of existing dwellings will also be significantly impacted due to the height and scale of this proposed development.

## Questions

- Has there been consideration of school zoning with the addition of 162 residential units in the area? EB Chandler and Spring Street Academy are already jammed full with students, where do extra children attend school? How does this impact the level of education that children already attending these schools receive?
- How will the additional traffic and noise in the area be handled?
- Will traffic policing be increased in the area?
- Is there sufficient staffing for emergency services coverage for such a development?
- Does Cumberland Regional Health Care Centre have staffing to cover the potential of 162 new families moving into the area?
- How will this new development impact property values and taxes in the area?

- The site plan illustrates 4 large propane tanks that will be positioned in close proximity to EB Chandler Junior High. Is this not a safety concern?

In closing, I appreciate the opportunity to submit comments and questions and look forward to continued dialogue about this development. I feel strongly that it changes the nature, character and safety of our community. I certainly hope that the Planning Advisory Committee and Town Council put the safety and comfort of residents ahead of any potential tax income that would come from such a development.

Thank you,

Jeff Smith

**RE: Lot 21-2R Apartment Complex**

**Received via email 20220202**

We received your invite in the mail and I guess our only concern is if the area will be fenced in or will the residents be walking willy nilly through peoples yards? These are not going to be low rental I hope.

We knew the land would be developed into some sort of apartments but we're expecting ones similar to the ones off Gladstone.

Thank you,

Barb March

**RE: Lot 21-2R Apartment Complex**  
**Received via email 20220120**

Questions and concerns re proposed multi-unit development re lot 2021-2R PID 25002122  
Brown/Mosher streets.

1. Why was this proposal not distributed to all residents on both sides of the following streets: Donald Avenue, Walter Purdy Avenue and Willow Street? Such a huge development will impact them in terms of increased vehicle traffic, pedestrian traffic, possible increase in infrastructure: sidewalks, water/ sewage development, more police presence, possible increase in unsanctioned activities ie: criminal.
2. What is the Town of Amherst going to do to control traffic in these areas above? Already traffic has increased especially on Willow Street, with increased noise and speeders (including school busses). With this development there is a potential of 210 cars using basically streets that are one car lanes save for Willow Street. {3 x 54 units = 162 + 1/3 of 162 = 50 may have a second car =210}.
3. Should the development go through, what is the Town's position with increased vehicle and pedestrian traffic?
4. The 162 units will increase the population in an established quiet and quaint neighbourhood. What is the Town and Developers going to do especially for families with young children and teens to keep them occupied and out of trouble?
5. What is the makeup of the residents expected?
6. Concern: With more people, it is expected some of these residents will have pets. It can be anticipated the pets owners will allow cats to roam free and dog owners to allow their dogs to defecate in the soccer field and surrounding area without picking up after their pets. Lots of soccer families and E.B Chandler school children use this field. Already in the area, cats are roaming free.
7. Will the Town or Developers build fences for the established residents in the area as it is expected development residents will want to 'short cut' through people's property.
8. Do established residents have a say in this proposal or is it an already done deal?
9. Are there no other sites that can withstand such a large development?
10. Concern: Is such a large influx of people and apartments a good fit of land development for the quaint and quiet established neighbourhood?
11. Concern: Will not a high density population in a small area encourage less health and well-being for the area?
12. Why such a large development in a quaint and quiet established neighbourhood?
13. What is the Town's and Amherst Fire Marshal's position and emergency plan to meet the needs of the local residents and proposed development should an evacuation need to take place i.e. fire?
14. Will the Town and Developers build areas of a green buffer zone(s) and beauty to accommodate all residents?
15. If there is an influx of families, will not this increase an already overburdened school system? What is the plan to handle this issue besides wait until it happens?
16. Concern: With increased traffic and population, already waste is thrown from vehicles to lie on the street/property and wind-blown garbage is left for others to pick up.

17. Is there a better 'win win' situation such as slab-on-grade type buildings as found behind the Pharmasave Amherst?
18. Is this a development that is consistent with any applicable comprehensive plan for the area? Is this a zone usage/change that is consistent for the area and for the development of public services?

W.R. Nicholls

**RE: Lot 21-2R Apartment Complex**  
**Received via email 20220126**

I wish to thank you for the opportunity to join the zoom session tonight and object to this development for following reasons:

Too many apartments for the space. There will be 162 units, 138 of them 2 bedrooms containing approx 350 residents, most of which will have cars, way to many for the parking spaces.

The auto traffic on Willow St and Donald ave will be unbelievable and fast considering three schools in this area.

I am glad there will be only a walkway and not a road to Donald Ave.

This town needs affordable housing and these units certainly will not be affordable to most residents.

These bldgs may have an adverse effect on our house value.

Charlie & Judy Tooke  
18 Donald Ave

**RE: Lot 21-2R Apartment Complex**  
**Received via email 20220127**

Hello and thank you for the opportunity to express our concerns over the proposed apt developments. As was spoken many times last evening we dread the lack of privacy these apts will present to our yards and even in windows. Apts above the first floor will be directly over our back yards thus reducing our privacy in our yards to none. Even construction of a fence will provide no privacy.

The traffic is another great concern as mentioned the kids driving cars on these streets at great speeds are a huge problem to which we have called town police a few times. There is simply not enough resources to monitor this continuously.

We have a lovely residential area with the added blessing of some open space that will become a concrete jungle.. This in all respect is not the city, we enjoy our quiet way of life..many cats hunt in these fields for survival , I have personally watched ducks in the brook when water runs high..birds make their home in our trees.. All reasons we purchased this home last year. Garbage at the school is allready a disgrace in the brook that runs there let's not add 160 odd apts to add to that problem. Has any review been taken to see if the citizens of Amherst can afford this rent.

Noone in our area or surrounding want or support the building of this nature here.As someone pointed out there are better spots to build that is not in the back yards of a block of citizens who.have strived to maintain a quiet place to live.

Will this drop the value of our homes? I fear we know the answer to this.

Also the power grid. As everyone knows we lose power on a regular basis here in amherst. This will only add to the pressure put on a weakening grid system.

I respect that a developer would want to build likely needed apts in our town but at the cost of people who have lived here for most of our lives.

Thank you for reading Kelly Gallant @

24 Donald Ave

**RE: Lot 21-2R Apartment Complex**  
**Received via email 20220127**

I live at 40 Donald Avenue, when we moved here just over four years ago, we thought we bought in a residential area that was designated for single dwelling homes. The proposal to put 3 large structural units in this area that would dwarf the surrounding homes seems to take away not only from the residential feel, but I believe brings down our property values. At various times during the day Donald Avenue is a very busy street with both vehicle traffic and school children. The added vehicle and foot traffic generated by the units on the narrow streets in the surrounding area will create a more dangerous environment. We have vehicles speeding up and down the street, especially when the Highschool gets out. At times during the day this is a noisy street, we have two bumps near my home that were created because of sewer repair and when cars and especially the heavy traffic hit them it's loud. The extra traffic will only increase the noise as will the construction of the units. As mentioned during the meeting last night is there no area in the town of Amherst more suited for these types of structures, someone did mention Robert Angus Drive. It's only my opinion but I don't believe these size units should be in this residential area of town.

Mike O'Brien  
40 Donald Avenue

**RE: Lot 21-2R Apartment Complex**  
**Received via email 20220128**

Good afternoon Kim,

Thank you for the opportunity to submit written comments to the Planning Advisory Committee (PAC) regarding the proposed apartment buildings in our neighborhood. Kudos to you for facilitating the virtual public session on January 26th. I have 6 points I would like to present in my comments.

1. Is there a need in Amherst for 162 new rental units? What is the vacancy rate in the apartments already in town? Many new families have relocated to our town in the past two years. However, these families and others who follow them are mostly buying single family dwellings.
2. The proposed four storey buildings would tower over the single family dwellings surrounding them. As mentioned in the virtual session the buildings would block the sun for many homeowners. Most of the homes bordering the apartments would suffer a lack of privacy as unit residents would be looking down on them from a height of four storeys. There are many apartments already in our section of town. If the buildings are deemed necessary perhaps they could be reduced to a maximum of two storeys. Placing such high buildings in a lot surrounded on three sides by detached one or two storey homes is not acceptable to the local community. Most buildings of this height are located at the edge of residential areas, not in the centre of them.
3. We were informed at the session there would be 1.2 parking spaces for each unit. Most couples today own two vehicles plus there are always visitors and delivery vehicles coming and going. For 162 units probably at least 300 parking spaces would be required. Where would all the extra vehicles park? On surrounding streets? Our property is on Brown Street. It is a narrow street. Would there be an overflow of parked vehicles infringing on our front lawn?
4. The main concern expressed at the virtual session was increased traffic. Presently there are three houses on Brown Street. We chose to build our new home in this area because it was such a quiet neighborhood. Our son is a wheelchair user and there are no sidewalks on the street. However, he safely wheels around the immediate area because there is low traffic. Having 300 plus vehicles traveling on Brown Street to access the apartment buildings behind us would drastically change that situation. Even half that number of vehicles would greatly increase the risk of pedestrian injury or death.
5. Having 162 additional families in such close proximity is another concern. Would there be an increase in noise? In vandalism? Would the existing families on the surrounding streets still be able to enjoy their own properties?
6. Having a complex of large apartment buildings in such close proximity would undoubtedly adversely affect the property values of the surrounding homes. Not only would that be detrimental to the property owners, but would result in reduced revenue for the town in property taxes.

In conclusion, we feel the proposed buildings are not a good fit for our neighborhood. If the town deems additional rental units are required in our area, perhaps a smaller project could be

considered. One that complements the surrounding homes and does not decrease the enjoyment and safety of others.

Evelyn and John Bradford  
3 Brown Street

**RE: Lot 21-2R Apartment Complex**

**Received via email 20220130 from Mary Jane Weeks**

Will, I still do not see any form to fill out so I will write how I feel about the new building.

This is a nice quiet Street to live on summer, spring, fall and, winter

Except they do not blow the street very wide, which will make a really big difference if 162 new family's move in. First you will have the noise from them building the 3 apartments building early in the morning to late at night. The traffic will be heavy on Walter Purdy Ave. and don't say they are building a new road, because you and I know they will be using Brown, Mosses and Walter Purdy Ave. also, and by the look of things you are also building a road through to Donald.

Now that is 162 new families which is a lot of new people to add to one neighborhood, 162 plus new cars in the neighborhood, so 162 with an average of 2 people per unit and that brings it up to 445 new people in one neighborhood. ( and that is just approximate amount of people ) that is a lot of people to bring in one neighborhood all at once. People on this street keep their places looking nice, sad to say you cannot not say that about apartment buildings.

Look you want to put in 4 by 4 units I call them senior units, that is fine but not 162 units.

There is lots of land around Amherst, what about out there by where steam boat was on the right side going out of town. Lots of

land there.

Now people on Donald, Walter Purdy and Willow st. all should had been notified about this built because it affects all of us not just a few.

**PLEASE DO NOT BUILD THE 162 UNITS.**

Weeks'

**RE: Lot 21-2R Apartment Complex**  
**Received via email 20220131**

Jeff & Marilyn Campbell - 8 Mosher Street

Our concerns:

- Has an environmental impact study been completed? What were the results?
- Has a traffic study been completed? What were the results?
- Mosher and Brown were not meant for that amount of traffic. They are small streets and in winter, poorly cleaned, and often down to one lane. Walter Purdy as well.
- Mosher, Brown, and Walter Purdy don't have sidewalks. As well as Willow between EB and the high school. With an increased amount of kids walking through, are sidewalks planned? How much property will that eat into?
- Parking: most apartments will have 2 or more cars per household. There is not enough parking for them. What is the plan for overflow parking? Brown, Mosher and Walter Purdy do not have room for overflow parking. There will be dual adult households as well as there will no doubt be Mount A students living 2-4 per apartment and having multiple cars. The 1.2 spots planned for these complexes can not accomodate that.
- Are these low income apartments like on Dickey Street and Willow? Crime rate will increase. Our property value will go down. Will there be compensation for that?
- Has the town researched other properties by this owner and developer? Are they consider "slum lord properties" or well taken care of?
- Where will the dumpsters be stored on the property? What is the plan to not attract rodents to the area?
- Spring Street Academy is already at max capacity and they've taken over the library to use as an extra classroom. Where will all these kids go to school? What about EB's capacity and ARHS?
- Has a policing impact study been done? Does APD have the manpower to cover off this influx of people? How will this effect their budget and town taxes?
- Has a firefighting impact study been done? Amherst FD is supported by volunteers and neighbouring volunteer fire departments. Are they trained to fight a fire in a 4 storey apartment building? Most fires fought by volunteers are fought in a "surround and drown" type response - this is not possible in a 4 storey apartment building. Do they all have the necessary equipment to fight a fire in a 4 storey apartment building? How will this effect their budget and town taxes?

- How will this effect water pressure? Donald seems to have frequent water line breaks. Is there capacity for this on the current system?
- Does NS Power have the capacity to supply this set-up? Our power tends to fail with any/every storm. I know through someone locally who works for NS Power that this area is on one of the oldest and most instable grids - 3 large apartment buildings would not help the issue.
- What about parks? A complex of this nature should have the responsibility of having playground equipment nearby. Otherwise kids will have nowhere to play. The closest areas to play would be behind SSA and Dickey park - which are not considered nearby for young kids to play.
- Bottom line: we were told when we built here that the neighbouring properties would consist of houses of similar style to ours or the duplex-style ones (mostly for seniors). We would never have built here if we knew we'd be on the direct exit for a 3-building apartment complex. If this is built, we will be moving for sure. I know of at least 3 households on Mosher, Brown and Walter Purdy that will sell and move. A realtor recently told us what the approximate selling price of our house would be - now with this in the works, that will no doubt decrease. Will Nova View Developments Ltd be compensating us for the loss in our selling potential? Honestly, if this is going forward, the developer might as well buy out all of Mosher and Brown, tear down the houses and put in a park and extra parking.

**RE: Lot 21-2R Apartment Complex**  
**Received via email 20220131**

Good afternoon Kim,

Thank you for the opportunity to attend your virtual meeting as well as submit written comments to the Committee regarding the 3 proposed apartment buildings in our neighborhood.

I live at 1 Walter Purdy Avenue and co-own the property at 3 Brown Street. I am very concerned about the extra traffic that these units will create.

We built the house on Brown Street for our sons with disabilities - one using a wheelchair and one with Down Syndrome and a lady with another disability. This property was more expensive than others in Amherst at the time but this has always been a quiet and safe neighborhood. This will undoubtedly change with an additional 300 - 400 people moving about. Safety is our number one priority.

I understand there is a need for new apartments in Amherst however, these are large intrusive proposed units. There has to be other property in Amherst where these buildings and additional people and vehicles would be more appropriate rather than in the middle of such a long-established single family home area where total privacy will be lost. I feel confident that this will lower the value of these homes. Personally, I have never and would never consider purchasing a property with apartment buildings overlooking it.

We have most of our young people from Amherst, as well as many foreign exchange students walking and/or driving to the three schools in our area. There is no doubt that the additional traffic will cause many safety issues.

I trust the Committee will consider our concerns when making their decision.

Thank you,

Nancy Park

**RE: Lot 21-2R Apartment Complex**  
**Received via email 20220131**

To whom this may concern;

Being a resident of Brown St for 8 yrs now I have concerns regarding this new phase that is being implemented.

As I'm sure the rest of my neighbours have expressed their opinions , I also stand together with them and not in favour for these apartment buildings.

- 1- this will depreciate our value of our homes
- 2- assuming for low income housing
- 3- why not relocate these buildings to LaPlanche st - that would be a great spot
- 4- why not build houses instead ?
- 5- assuming our opinions won't matter on this topic, what is the date this will be going through ?

Thank for entertaining my opinion, again I am 100% NOT in favour for these apartments to be built in my neighborhood.

Sincerely  
Tanya Musseau

**RE: Lot 21-2R Apartment Complex**  
**Received via email 20220201**

Dear Sir or Madam,

I am writing to voice my concerns about the proposed Lot 21-2R Apartment Complex PID 25002122 Brown and Mosher Streets.

I am a resident and property owner at 2 Walter Purdy Ave. I have the following points of concern regarding the proposed development noted above:

1. Safety & Traffic - At present, Walter Purdy Ave. is already used as a thoroughfare by drivers who wish to bypass heavier traffic on Spring St. and East Pleasant St. Drivers are frequently in a hurry, rolling through stop signs, taking turns too tightly and driving too fast for the narrow, winding street. Our snow banks are often enormous and in the winter our narrow street is inevitably more narrow due to the real-life limitations of snow removal. Young children play in their front yards during all seasons and people park their vehicles along the road due to limited driveway space, which results in near-miss accidents on a regular basis. Walter Purdy Ave., Brown St., and Mosher St. are simply not equipped to handle the increase in traffic that a 3 building apartment complex would cause.
2. Space - The Lot is simply too small to comfortably house the proposed apartment complex. The proposed plans literally cram the buildings into the space with zero room for flexibility/leeway (i.e., green space, snow removal, etc.). This leads me to believe that the proposal is 100% financially motivated, and does not consider the well-being of the occupants, the neighbourhood, or the town overall.
3. Educational Impact - As mentioned above, the proposal places Building B a mere 12 metres (39 feet) away from EB Chandler Jr. High School. As an educator and school psychologist (R. Psych.), I have significant concerns regarding a multi-storey building being that close to a school. Students who attend EB Chandler are in grades 7 and 8 - these are challenging years for children, not only academically but also developmentally. During these 2 years, every child in the Amherst area attends this school, and it is vital that they are able to secure strong connections with their teachers and friends, and sustain focus/concentration on their academic learning. Not only will the construction of the proposed buildings result in noise and visual distractions for students, but once completed the reality of housing hundreds of people only a few metres away is going to create ongoing barriers to their learning (e.g., residents are free to play music at a high volume with their windows open, work on their car engine in the parking lot, smoke marijuana on their balcony, etc. - all well within view/earshot of hundreds 11-13 years olds in the middle of the school day).
4. Infrastructure - What guarantee is the town of Amherst able to make that our water, sewer, snow removal, electricity, and internet systems are able to manage such a large increase in usage with respect to infrastructure? As it is, we have regular power interruptions and internet connectivity is barely satisfactory.
5. Neighbourhood Aesthetic - My husband and I chose to purchase a home on our street and in this neighbourhood because of the relative newness of the houses, the single-family nature of the homes, and the residential/suburban feel to the area (i.e., lots of green space, well-kept homes/yards, no tall buildings to be seen, etc.). While I can appreciate the desire to increase housing options, I feel as though single-family homes or duplex-style homes would be a better fit for this area, and would still meet the housing needs of many people looking for places to live near schools, parks, and friends. A multi-storey apartment complex in the middle of our neighbourhood would surely be an eyesore, not only from the

start but also as time goes on given the difficulty of properly maintaining such large structures and their surrounding landscapes without enormous financial commitment.

6. Property Value - Apartment complex housing will decrease my property value, which is a significant concern.

My husband and I are not originally from the Amherst area but we happily moved here and purchased our home on Walter Purdy in August 2008 after being offered jobs with the local school board. We love this town and are proud to call it home. The development of Lot 21-2R as it is currently proposed has created a lot of worry and resulting uncertainty for us. I sincerely hope the many concerns of our fellow neighbours will be taken into thoughtful consideration.

Many thanks for your time and consideration,  
Lisa Craig

**RE: Lot 21-2R Apartment Complex**  
**Received via email 20220201**

Dear Sir or Madam,

I am writing with regards to the proposed Lot 21-2R Apartment Complex PID 25002122 Brown and Mosher Streets.

I am a resident and property owner at 2 Walter Purdy Avenue and I have the following points of concern regarding the proposed development noted above:

1. Safety & Traffic - There are many children who currently live on Walter Purdy Ave., Brown St. and Mosher St. These children are often found playing or riding bikes on these streets. At present, Walter Purdy Ave. is already used as a thoroughfare by drivers who wish to bypass heavier traffic on Spring St. and East Pleasant St. Walter Purdy Ave., Brown St., and Mosher St. are simply not equipped to handle the increase in traffic that a 3 building apartment complex would cause.
2. Educational Impact - The proposal places Building B a mere 12 metres (39 feet) away from EB Chandler Jr. High School. As a teacher at EB Chandler, I have significant concerns regarding a multi-storey building being that close to a school. Students who attend EB Chandler are in grades 7 and 8 - these are challenging years for children, not only academically but also developmentally. During these 2 years, every child in the Amherst area attends this school, and it is vital that they are able to secure strong connections with their teachers and friends, and sustain focus/concentration on their academic learning. Not only will the construction of the proposed buildings result in noise and visual distractions for students, but once completed the reality of housing hundreds of people only a few metres away is going to create ongoing barriers to their learning (e.g., residents are free to play music at a high volume with their windows open, work on their car engine in the parking lot, smoke marijuana on their balcony, etc. - all well within view/earshot of hundreds 11-13 years olds in the middle of the school day).
3. Infrastructure - What guarantee is the town of Amherst able to make that our water, sewer, snow removal, electricity, and internet systems are able to manage such a large increase in usage with respect to infrastructure? As it is, we have regular power interruptions and internet connectivity is barely satisfactory.
4. Neighbourhood Aesthetic - My wife and I chose to purchase a home on our street and in this neighbourhood because of the relative newness of the houses, the single-family nature of the homes, and the residential/suburban feel to the area (i.e., lots of green space, well-kept homes/yards, no tall buildings to be seen, etc.). While I can appreciate the desire to increase housing options, I feel as though single-family homes or duplex-style homes would be a better fit for this area, and would still meet the housing needs of many people looking for places to live near schools, parks, and friends. A multi-storey apartment complex in the middle of our neighbourhood would surely be an eyesore, not only from the start but also as time goes on given the difficulty of properly maintaining such large structures and their surrounding landscapes without enormous financial commitment.
5. Property Value - Apartment complex housing will decrease my property value, which is a significant concern.

My wife and I are not originally from the Amherst area but we happily moved here and purchased our home on Walter Purdy in August 2008 after being offered jobs with the local school board. We love this town and are proud to call it home. The development of Lot 21-2R as it is currently proposed has

created a lot of worry and resulting uncertainty for us. I sincerely hope the many concerns of our fellow neighbours will be taken into thoughtful consideration.

Many thanks for your time and consideration,

Wade LeBlanc

**RE: Lot 21-2R Apartment Complex**

**Received via email 20220202**

Good Day

My name is Brad Delahunt I live on Donald Ave. I am very concerned about the proposal of a 54 unit apartments . The safety of my children walking to school. Increase crime in the neighborhood. With the apartments puts a lot more cars on the road. The schools are over crowded now. Where are all the kids going to go to school. The apartments are going to be an eye sore. My family chose to live in Amherst because it's a small town. If I wanted to look at apartments I would live in Halifax.

I watched your video on YouTube. No one answered one of the more important question. Is the apartments going to be low income? Could you send me the minutes of the meeting that was held on January 25 2022?

Thank you

Brad Delahunt

**RE: Lot 21-2R Apartment Complex**  
**Received via email 20220131**

To Whom it May Concern,

I am writing today with my concerns for the proposed apartments in the Donald Ave and Willow St areas.

My first concern as addressed during the public information session is the fact the proposed development is going to create a lot of excess traffic in an area that is already very congested during the school days. The amount of traffic in the Donald, Dickey and Willow street areas are a safety hazard on the best of days, let alone adding parking for almost 200 to the area. The amount of close calls and near misses is a frequent concern already along with the speed of traffic in the school zones.

My next concern is the size of the proposal! Living at 22 Donald Ave and having teenage daughters I worry about the privacy not only for neighbours who will have people looking into their windows and backyards from their balconies taking away all privacy but also for EB Chandler Jr High! Having this in the back of the school will take privacy away from the schools, it will create significant noise and these schools rely on opening their windows quite often as that school is older and a very warm building, as well as windows being open during Covid times. I feel the noise from construction as well as after construction would be very disturbing to the education of these child and create a harder learning environment.

My next concern is taking away of more green space in the area and the wildlife that currently occupy the area. There are many areas complexes like this could be built but right in the middle of a quiet residential area and in the back yard of a school is not the place.

Then there is the concern for safety. I do not feel a congested area like this would be ideal I'm the backyard of a school with 12-14 year olds. That is a lot of people with access to the school yard at all times, the small school grounds that currently has children in it all of the time. How can we ensure people are remaining off of the school property away from the children?

My next concern is for the schools, currently Spring St Academy is over capacity, they lost their library to make another classroom and grade 5&6 French immersion are bussed to another school because SSA can't handle capacity , a building of this size is surely going to have additional families move to this area and how is the elementary school expected to handle this.

Another concern I have is snow plowing in the winter months. Being that my property line is by one of the proposed parking lots I would like clarification on where the snow will be plowed in the winter months. I have children as do many neighbours that play in the back yards and I am not wanting the snow hills to be pushed back into my back yard but based on the layout I am unsure where else it could go. We get hefty snowfalls and that would also be a major concern.

Currently in Amherst housing is a major concern for many residents and the cost of living has jump significantly, I do not feel this looks like affordable housing and as someone mentioned how will this affect our already unstable power grid in the area on any given windy day, is the town prepared in make the surrounding streets a no parking on streets permitted area and enforce it because of the dangerous surrounding the schools when all of the cars that don't fit in the parking lot need an area to park, will

the APD be hiring additional members to ensure the safety of the kids who already come close to getting hit daily in the already busy area.

I am not opposed to the area being developed, however 3 buildings of this size in such a small quiet residential area and in a schools back yard is not in the best interest of anyone.

Amherst isn't a city and these structures are something you would find in an urban setting not in a small rural town.

I am asking you to not approve this proposal for the safety of the kids in the area! I feel this is not in the best interest of the kids or the residential area.

Thank you,

Melanie Sealy

# Lot 2021-2R Amherst Traffic Impact Study

July 2022

Prepared for  
NovaView Developments



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Prepared by

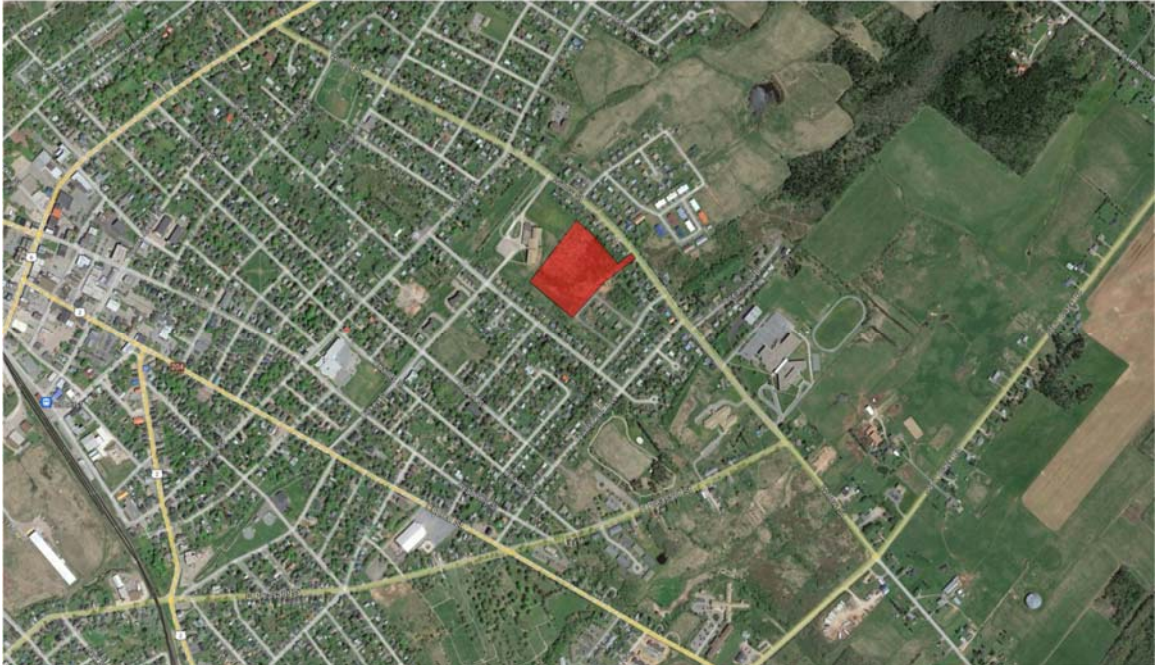
Jeff R. LeBlanc, P.Eng., PMP

# 1 Introduction

## 1.1 Background

NovaView Developments is working on a proposal to develop their property, identified as Lot 2021-2R, with three 54-unit apartment buildings in Amherst, Nova Scotia. Exhibit 1.1 shows the site in red in the context of the surrounding area in Amherst, Nova Scotia

Exhibit 1.1 – Lot 2021-2R in Amherst, Nova Scotia



Source: Google Earth

The site plan provided for the proposed development shows three new 54-unit residential apartment buildings with a total of 162 units with primary access from an extension of the existing Gallagher Street to Willow Street. One driveway to the property will be established from this extension and a second access point will be created at the western end of Gallagher Street at Brown Street. A new internal road will be constructed on the property to provide access to the apartment buildings and three surface parking areas that will contain 204 parking spaces. An emergency vehicle turnaround has been included on site.

The proposed development is located adjacent to EB Chandler Junior High School and approximately 500 meters west of Amherst Regional High School. The property is under the jurisdiction of the Town of Amherst and the surrounding transportation network is owned and maintained by the Town of Amherst.

Refer to Exhibit 1.2 for a view of the proposed development and surrounding road network, Exhibit 1.3 for a proposed site plan, Exhibit 1.4 for a typical floor plan and Exhibit 1.5 for renderings of a typical apartment building exterior as prepared by Paul Skerry Architects Ltd and as provided by NovaView Developments.

Exhibit 1.2 – Proposed Lot 2021-2R Development and Surrounding Road Network in Amherst



Exhibit 1.3 – Lot 2021-2R Proposed Site Plan

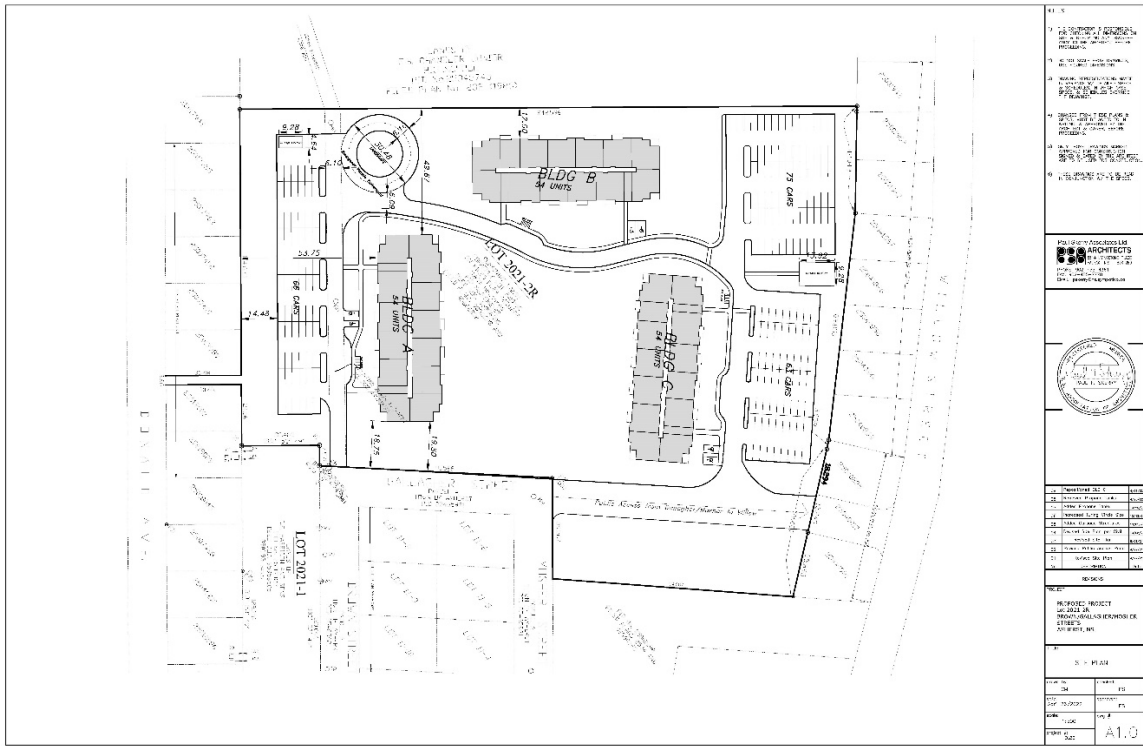


Exhibit 1.5 – Lot 2021-2R Typical Apartment Building Exterior Renderings



The Town of Amherst has reviewed the initial development application and they have requested that a Traffic Impact Statement be completed to *“estimate trip generation and the impact on traffic levels on the surrounding street network, including: Willow Street, Donald Avenue, Brown Street, Mosher Street, Walter Purdy Drive, and the new connection to Willow Street from the development. The statement will address the extent to which the traffic generated from the proposed development will impact the performance of streets and intersections in the vicinity in relation to the current standards for traffic volumes on local and arterial streets, as set out in the ‘TAC Manual’. If estimated trip generation exceeds surrounding capacity, estimate the number of added dwelling units that would not result in exceeding the standard.”*

We are pleased to submit this report that addresses the Town of Amherst comments above and also includes our findings and recommendations.

## 1.2 Study Area

The study area defined for this Traffic Impact Study consists of the transportation network in the immediate vicinity of the proposed development of Lot 2021-2R including the new connection on Willow Street along with the follow streets and key intersections:

- Donald Avenue at Walter Purdy Avenue
- Brown Street at Walter Purdy Avenue
- Mosher Street at Walter Purdy Avenue
- Willow Street at Walter Purdy Avenue
- Site Access (Gallagher Street Extension) at Willow Street

We have set a horizon period of 5 years for this development to allow it to be fully completed and occupied so we can assess future performance including applicable annual background traffic growth in the area.

## 1.3 Objectives

Given the background set out above, the objective of this Traffic Impact Study is to assess the impacts of the proposed development on the surrounding transportation network. Recommendations and solutions will then be provided to allow the traffic generated by the proposed development to be introduced to the existing transportation network safely and efficiently.

Detailed objectives are presented below:

- Complete a Site Review to review road width, lane markings, stopping sight distances, pedestrian facilities, active transportation and roadway signage within the study area
- Complete new AM and PM peak hour manual traffic counts at these intersections:
  - Willow Street at Walter Purdy Avenue
  - Donald Street at Walter Purdy Avenue
- Analyze the performance (2022) of the following key intersections using Synchro plus SimTraffic Version 10 and the procedures outlined in the Highway Capacity Manual and by the Transportation Association of Canada (TAC)
  - Willow Street at Walter Purdy Avenue
  - Donald Avenue at Walter Purdy Avenue
  - Brown Street at Walter Purdy Avenue
  - Mosher Street at Walter Purdy Avenue
- Review historic 24-hour traffic volumes from NSTIR to estimate annual background traffic growth in the area
- Analyze the future background traffic performance (2027) of key intersections using Synchro plus SimTraffic Version 10 and the procedures outlined in the Highway Capacity Manual and by the Transportation Association of Canada (TAC)
- Estimate the amount of traffic to be generated by the proposed development of 162 apartments using Trip Generation Rates and equations published by the Institute of Transportation Engineers (11<sup>th</sup> Edition)
- Distribute new site generated traffic to the network based on observed traffic distribution from manual traffic counts

- Analyze the future performance (2027) of the following key intersections including site generated traffic using Synchro plus SimTraffic Version 10 and the procedures outlined in the Highway Capacity Manual and by the Transportation Association of Canada (TAC)
  - Willow Street at Walter Purdy Drive
  - Donald Avenue at Walter Purdy Drive
  - Brown Street at Walter Purdy Drive
  - Mosher Street at Walter Purdy Drive
  - Site Access (Gallagher Street Extension) at Willow Street
- Assess the need for right and left turn auxiliary lanes at the proposed entrance to the development from Willow Street (Gallagher Street Extension)
- Complete a visual assessment of Stopping Site Distance at the proposed access from Willow Street (Gallagher Street Extension)
- Develop and list existing, potential and future problems or concerns along with recommendations to safely manage site generated traffic

## 2 Existing Traffic Conditions

### 2.1 Description

The principal routes affected by this proposed development are Willow Street, Walter Purdy Avenue, Donald Street, Brown Street, Mosher Street and Gallagher Street in Amherst, Nova Scotia. The proposed development is located adjacent to EB Chandler Junior High School and approximately 500 meters west of Amherst Regional High School.

Although this development is in the Town of Amherst we have included Halifax Regional Municipality's (HRM) Characteristics of Street Classes from HRM's Municipal Service System Guidelines in Exhibit 2.1 as a reference and we have also included Transportation Association of Canada's (TAC) Characteristics of Urban Roads in Exhibit 2.2.

Exhibit 2.1 – HRM's Characteristics of Street Classes

Characteristic	Arterial Street	Major Collector	Minor Collector	Local Industrial	Local Street
1. Traffic Service Function	First Consideration	Traffic movement primary consideration, land access secondary consideration, some parking	Traffic movement of equal importance with land access, parking permitted	Traffic movement secondary consideration with land access primary consideration, parking permitted	Traffic movement secondary consideration with land access primary consideration, parking permitted
2. Land Access Function	Limited Access with no parking				
3. Range of design traffic average daily volume	More than 20,000	12,000 to 20,000 or more	Up to 12,000	Less than 3,000	Less than 3,000
4. Characteristics of traffic flow	Uninterrupted flow except at signals; w/ pedestrian overpass	Uninterrupted flow except at signals and crosswalks	Interrupted flow	Interrupted flow	Interrupted flow
5. Average running speed in off-peak conditions	50-70 km/hr	40-60 km/hr	30-50 km/hr	15-30 km/hr	15-30 km/hr
6. Vehicle types	All types	All types but trucks may be limited	All types with truck limitation	All types	Passenger and service vehicles, transit buses; large vehicles restricted
7. Connects to	Expressways, arterials, major collectors, minor collectors	Expressways, arterials, major collectors, minor collectors, some locals	Arterials, major collectors, minor collectors, locals	Some major collectors, minor collectors, locals	Some major collectors, minor collectors, locals

Exhibit 2.2 – Transportation Association of Canada Characteristics of Urban Roads

	Public Lanes		Locals		Collectors		Arterials		Expressways	Freeways
	Residential	Commercial	Residential	Indust./Comm.	Residential	Indust./Comm.	Minor	Major		
traffic service function	traffic movement not a consideration		traffic movement secondary consideration		traffic movement and land access of equal importance		traffic movement major consideration	traffic movement primary consideration	traffic movement primary consideration	optimum mobility
land service / access	land access only function		land access primary function		traffic movement and land access of equal importance		some access control	rigid access control	no access	no access
traffic volume (veh/day) (typical)	<500	<1000	<1000	<3000	<8000	1000 – 12 000	5000 – 20 000	10 000 – 30 000	>10 000	>20 000
flow characteristics	interrupted flow		interrupted flow		interrupted flow		uninterrupted flow except at signals and crosswalks		uninterrupted flow except at signals	free-flow (grade separated)
design speed (km/h)	30 - 40		30 - 50		50 - 80		50 - 70	60 - 100	80 - 110	80 - 120
average running speeds (km/h) (off-peak)	20- 30		20 - 40		30 - 70		40 - 60	50 - 90	60 - 90	70 - 110
vehicle type	passenger and service vehicles	all types	passenger and service vehicles	all types	passenger and service vehicles	all types	all types	all types up to 20% trucks	all types up to 20% trucks	all types up to 20% trucks
desirable connections	public lanes, locals		public lanes, locals, collectors		locals, collectors, arterials		collectors, arterials, expressways, freeways		arterials, expressways, freeways	arterials, expressways, freeways
transit service	not permitted		generally avoided		permitted		express and local buses permitted		express buses only	express buses only
accommodation of cyclists	no restrictions or special facilities		no restrictions or special facilities		no restrictions or special facilities		lane widening or separate facilities desirable		prohibited	prohibited
accommodation of pedestrians	pedestrians permitted, no special facilities		sidewalks normally on one or both sides	sidewalks provided where required	sidewalks provided both sides	sidewalks provided where required	sidewalks may be provided, separation for traffic lanes preferred		pedestrians prohibited	pedestrians prohibited
parking (typically)	some restrictions		no restrictions or restrictions one side only		few restrictions other than peak hour		peak hour restrictions	prohibited or peak hour restrictions	prohibited	prohibited
min. intersection spacing <sup>1</sup> (m)	as needed		60		60		200	400	800	1600 (between interchanges)
right-of-way width (m) (typically)	6 - 10		15 - 22		20 - 24		20 <sup>2</sup> - 45 <sup>3</sup>		>45 <sup>3</sup>	>60 <sup>3</sup>

**Willow Street** is a two-lane arterial road that runs in an east-west direction from Victoria Street (Highway 6) to Route 204 in Amherst, Nova Scotia. It provides access to homes, businesses, apartments and schools and is a major transportation link in the town. There is a concrete sidewalk on its northern side near the proposed development and bike lanes are marked in both directions. The posted speed limit is 50 km/hr with a reduction to 30 km/hr in school zones to the east and west of the proposed development when children are present. Marked crosswalks are installed at the bus entrance to EB Chandler Junior High School and at Townshend Avenue.

**Donald Street** is a local/collector road that runs east-west between Spring Street and Townshend Avenue. There is a concrete sidewalk on its northern side. The posted speed limit is 50 km/h with a reduction to 30 km/h in the EB Chandler Junior High School Zone when children are present. Marked crosswalks are installed at the entrance to EB Chandler Junior High.

**Walter Purdy Avenue** is a short, local road (275 meters) that runs north-south between Willow Street and Donald Avenue. It provides access to single family homes. The posted speed limit is 50 km/h and there are no sidewalks.

**Brown Street** is a short, local road (140 meters) that runs east-west between Walter Purdy Avenue and Gallagher Street. It provides access to 3 single family homes. The posted speed limit is 50 km/h and there are no sidewalks.

**Mosher Street** is a short, local road (140 meters) that runs east-west between Walter Purdy Avenue and Gallagher Street. It provides access to 4 single family homes. The posted speed limit is 50 km/h and there are no sidewalks.

**Gallagher Street** is a short, local road (75 meters) that runs north-south between Brown Street and Mosher Street. The posted speed limit is 50 km/h and there are no sidewalks.

Refer to Exhibit 2.3 for photos of the Study Area.

Exhibit 2.3 – Study Area Photos



Lot 2021-2R Proposed Access from Willow Street



Lot 2021-2R viewed from Brown Street/Gallagher Street



Brown Street looking east to Walter Purdy Avenue at Gallagher Street



Gallagher Street looking north with proposed Development on left



Gallagher Street looking south with proposed development on right



Existing northern end of Gallagher Street to be extended to Willow Street



Moshier Street looking east to Walter Purdy Avenue at Gallagher Street



Brown Street at Walter Purdy Avenue looking east



Brown Street at Walter Purdy Avenue looking west



Walter Purdy Avenue at Brown Street looking south to Donald Street



Walter Purdy Avenue at Brown Street looking north



Walter Purdy Avenue at Moshier Street looking south to Brown Street



Moshers Street at Walter Purdy Avenue looking east



Moshers Street at Walter Purdy Avenue looking west to Gallagher Street



Walter Purdy Avenue at Donald Street looking south



Walter Purdy Avenue at Donald Street looking north



Donald Street at Walter Purdy Avenue looking east



Donald Street at Walter Purdy Avenue looking west



Walter Purdy Avenue at Willow Street looking north



Walter Purdy Avenue at Willow Street looking south



Willow Street at Walter Purdy Avenue looking east



Willow Street at Walter Purdy Avenue looking west



Willow Street at Proposed Site Access looking east



Willow Street at Proposed Site Access looking west

## 2.2 Existing Traffic Volumes

We completed AM (7am to 9am) and PM (4:00pm to 6:00pm) manual turning movement counts at the following intersections on Monday June 20, 2022

- Willow Street at Walter Purdy Avenue
- Donald Street at Walter Purdy Avenue

The AM peak hour is from 8:00 AM to 9:00 AM and the PM peak hour is from 4:15 PM to 5:15 PM

These counts at either end of Walter Purdy Avenue also allowed us to estimate existing traffic at these additional intersections in the study area.

- Brown Street at Walter Purdy Avenue
- Mosher Street at Walter Purdy Avenue

There are 21 single family homes on Walter Purdy Avenue/Brown Street/Mosher Street and we compared vehicle trip estimates from Institute of Transportation Engineers (ITE) Land Use 210 Single Family to observed traffic volumes entering and exiting Walter Purdy Avenue. Observed traffic in the AM peak hour was 19 vehicles higher (enter and exit) than ITE rates which shows that Walter Purdy Avenue is not a major shortcut and the additional traffic in the morning is likely related to schools in the area. The PM peak hour is outside of school dismissal and observed traffic is almost identical to ITE estimated traffic volumes. Refer to Exhibit 2.4.

Exhibit 2.4 – Observed Traffic Volumes vs ITE Estimated Vehicle Trips on Walter Purdy Avenue

LAND USE	QUANTITY	AM PEAK			PM PEAK		
		TOTAL	ENTER	EXIT	TOTAL	ENTER	EXIT
Single Family ITE Land Use 210	21	18	26%	74%	24	63%	37%
			5	13		14	8
Observed Traffic Volumes	21	37	43%	57%	23	61%	39%
			16	21		14	9
<b>VARIANCE</b>		<b>19</b>	<b>11</b>	<b>8</b>	<b>1</b>	<b>-</b>	<b>1</b>

We also reviewed estimated ITE vehicles trips for the 7 single family homes on Brown Street and Mosher Street to estimate traffic at their intersection with Walter Purdy Avenue. Refer to the following exhibits for existing traffic volumes in the study area.

Exhibit 2.5 – Donald Street at Walter Purdy Avenue Existing Traffic 2022

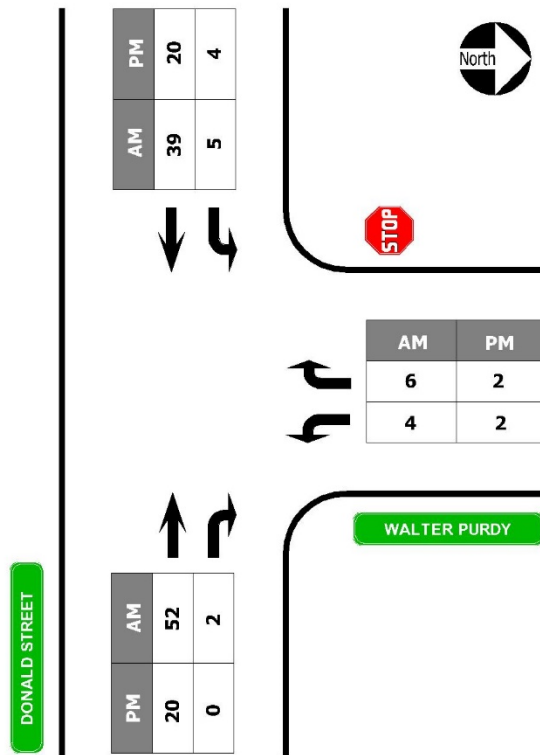


Exhibit 2.6 – Brown Street at Walter Purdy Avenue Existing Traffic 2022

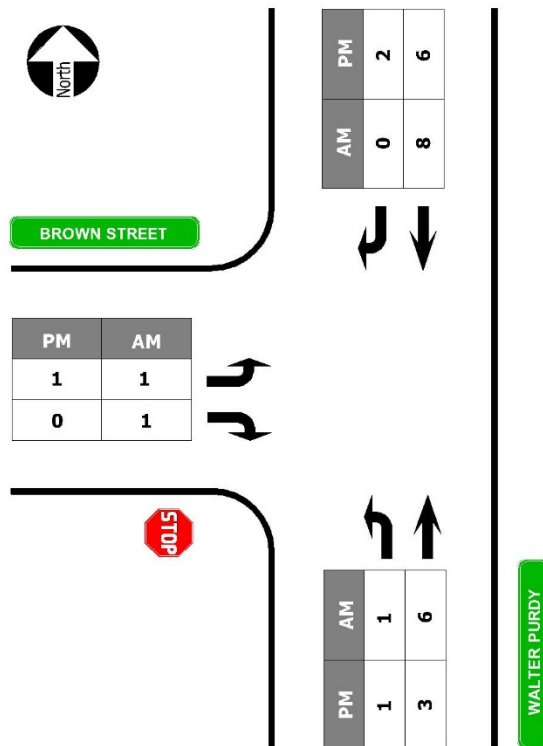


Exhibit 2.7 – Mosher Street at Walter Purdy Avenue Existing Traffic 2022

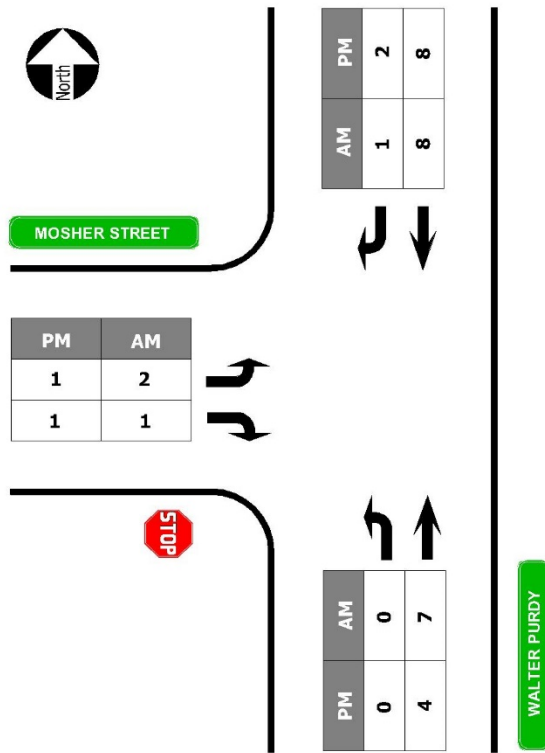
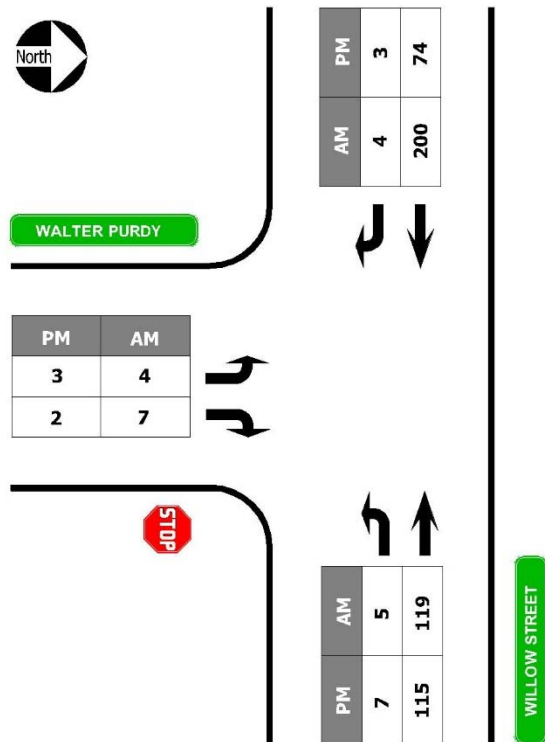


Exhibit 2.8 – Willow Street at Walter Purdy Avenue Existing Traffic 2022



### 2.3 Existing Trip Distribution

The AM peak hour had more traffic than the PM peak hour due to the impact of nearby schools and traffic on Willow Street was significantly higher than traffic observed on Donald Street and Walter Purdy Avenue.

In the AM peak hour we observed that 62% of traffic moves in an eastbound direction on Willow Street and the balance moves westbound. This reverses in the PM peak hour with 39% of traffic on Willow Street moving in an eastbound direction and 61% moving westbound

### 2.4 Background Changes in Traffic Conditions

We reviewed historical 24-hour Average Annual Daily Traffic (AADT) counts provided by NSTIR in the Amherst area and there was no clear pattern of background traffic growth over the past 20 years. Refer to Exhibit 2.9.

Exhibit 2.9 – NSTIR AADT Traffic Counts in Amherst, Nova Scotia

204-010		6-010		Tk2-240	
YEAR	AADT	YEAR	AADT	YEAR	AADT
1971	730	1973	2440	1971	2380
1972	740	1974	2460	1972	3410
1976	820	1976	2890	1974	3570
1977	960	1977	2540	1976	3770
1978	950	1982	3260	1980	3650
1980	1380	1983	2660	1982	3100
1982	1140	1984	3220	1986	3340
1986	1330	1986	3300	1988	3940
1988	1210	1988	4090	1991	3500
1991	1420	1991	4060	1995	3400
1995	1820	1995	2970	1998	3930
1998	2430	1998	2090	2001	3700
2001	2200	2001	3470	2004	3580
2004	2070	2004	2760	2007	2650
2007	1840	2007	1980	2010	4080
2011	2830	2016	2530	2013	3920
2013	1710	2019	3600	2016	3490
2014	2580			2019	3580
2016	2000				
2019	2380				

For this study, we applied an annual background traffic volume growth rate of 2% to observed 2022 traffic volumes as outlined in Section 2.2 to establish baseline traffic volumes in 2027 for analysis. Refer to Exhibits 2.10, 2.11, 2.12 and 2.13 for estimated 2027 background traffic volumes at all key intersections.

Exhibit 2.10 – Donald Street at Walter Purdy Avenue Estimated Background Traffic 2027

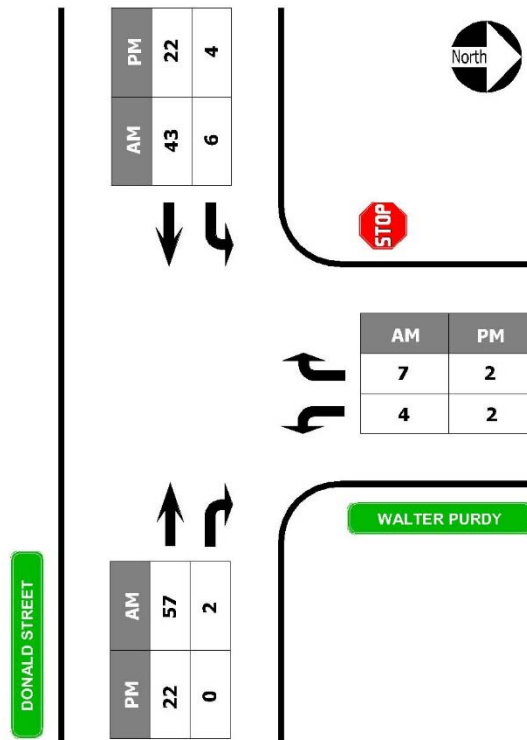


Exhibit 2.11 – Brown Street at Walter Purdy Avenue Estimated Background Traffic 2027

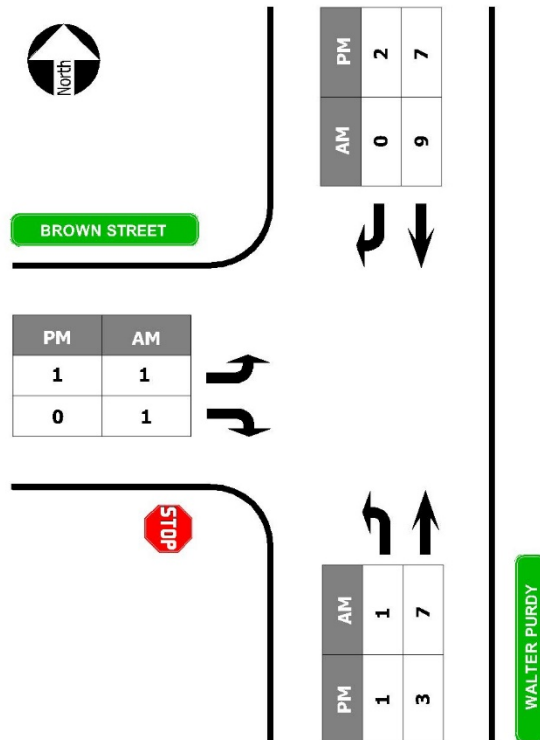


Exhibit 2.12 – Mosher Street at Walter Purdy Avenue Estimated Background Traffic 2027

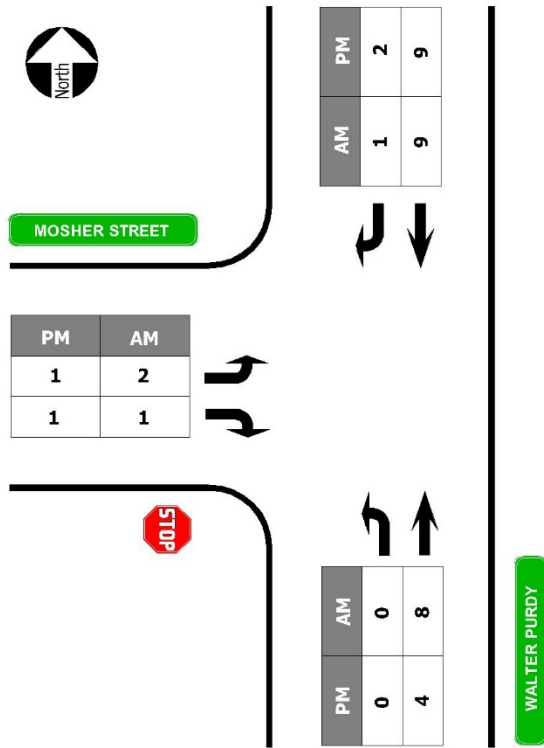
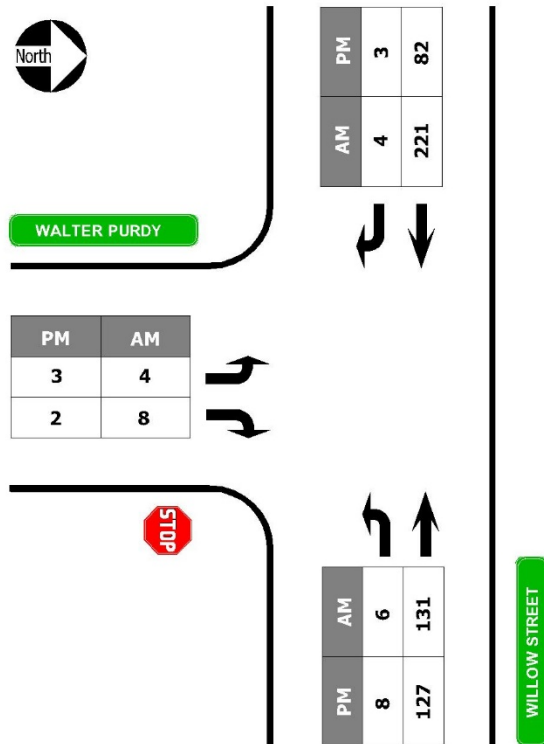


Exhibit 2.13 – Willow Street at Walter Purdy Avenue Estimated Background Traffic 2027



## 2.5 Transit, Pedestrians and Active Transportation

There is no transit in the area of the proposed development, however, with two schools nearby we did observe a number of school buses on Willow Street in the AM peak hour.

The pedestrian network in the area has been well established with sidewalks on the north side of Willow Street and Donald Street to provide safe access for walking. Marked crosswalks are installed on Willow Street at the bus entrance to EB Chandler Junior High School and at Townshend Avenue. Crosswalks are also installed at the entrance to EB Chandler Junior High School on Donald Street.

Willow Street was upgraded to include bike lanes on the both sides contributing to a pedestrian and bicycle friendly area which helps residents bike, walk and use other human power ways to move around the city.

In our manual traffic counts completed on Monday June 20<sup>th</sup> we observed 14 pedestrians in the AM peak hour using the sidewalk north of Willow Street and 1 pedestrian in the PM peak hour.

At the other intersection we observed 11 pedestrians in the AM peak hour using the sidewalk north of Donald Street and 1 pedestrian in the PM peak hour. These pedestrians cross Walter Purdy Avenue so they would have a minor impact on traffic entering or exiting this street.

For the detailed analysis in this report we have modeled 10 persons per hour for all intersection approaches in the study area.

### 3 Site Generated Traffic

#### 3.1 Trip Generation

The proposed development will have 162 apartments in three separate four-storey buildings. We completed trip generation estimates using equations provided in Institute for Transportation Engineer’s Trip Generation Manual 11<sup>th</sup> Edition.

- ITE Land Use 221 Multifamily Housing (Mid-Rise)

*"Mid-rise multifamily housing includes apartments and condominiums located within the same building that has between four and 10 floors of living space. Access to individual dwelling units is through an outside building entrance, a lobby, elevator, and a set of hallways."* The unit of measurement for average vehicle trip ends is dwelling units.

Exhibit 3.1 – Estimated Site Generated Traffic Volumes for Proposed Development

LAND USE	QUANTITY	AM PEAK			PM PEAK			WEEKDAY		
		TOTAL	ENTER	EXIT	TOTAL	ENTER	EXIT	TOTAL	ENTER	EXIT
Apartments ITE Land Use 221	162	60	26%	74%	64	61%	39%	726	50%	50%
			16	44		39	25		363	363
<b>TOTAL</b>		<b>60</b>	<b>16</b>	<b>44</b>	<b>64</b>	<b>39</b>	<b>25</b>	<b>726</b>	<b>363</b>	<b>363</b>

We estimate that the proposed development will generate net new traffic volumes of **60** vehicles in the AM peak hour, **64** vehicles in the PM peak hour and **726** vehicles on a weekday.

#### 3.2 Trip Distribution and Assignment

We distributed and assigned the site-generated trips to the transportation network by analyzing our manual traffic counts and associated trip distribution.

In the AM peak hour we calculated that 77% of traffic in the area is captured on Willow Street with the balance of 23% on Donald Street. In the PM peak hour 82% of traffic in the area is captured on Willow Street and 18% on Donald Street. We used these percentages to distribute site generated traffic in the area. The proposed connection to Willow Street is a direct route for traffic and we don’t see site generated traffic heading towards Willow Street using Brown Street, Mosher Street or Walter Purdy Avenue. We also expect that most site generated traffic heading to/from Donald Street to use Brown Street as it will connect to the 2<sup>nd</sup> site access driveway.

#### 3.3 Total Traffic

The estimated distributed site-generated traffic was added to the estimated existing 2027 background traffic volumes to obtain the total estimated future traffic volumes at all key intersections in the study areas including the new connection to Willow Street from the extension of Gallagher Street to the proposed development. Please refer to the following Exhibits for a summary of total traffic volumes in 2027 and the Appendix for detailed summaries that show how site-generated traffic was distributed to and from the Lot 2021-2R residential development.

Exhibit 3.2 – Donald Street at Walter Purdy Avenue Estimated Total Traffic 2027

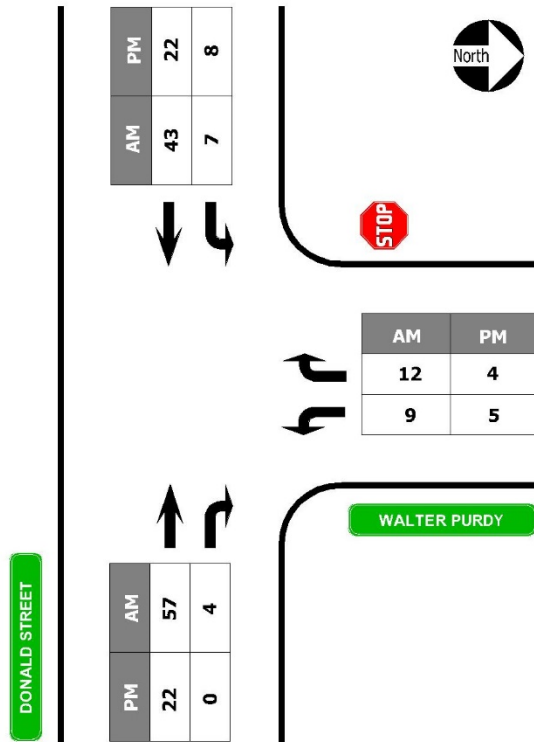


Exhibit 3.3 – Brown Street at Walter Purdy Avenue Estimated Total Traffic 2027

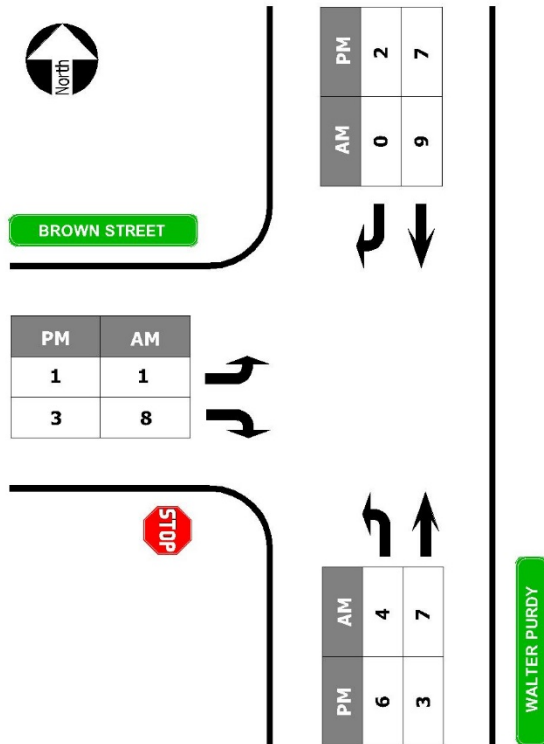


Exhibit 3.4 – Mosher Street at Walter Purdy Avenue Estimated Total Traffic 2027

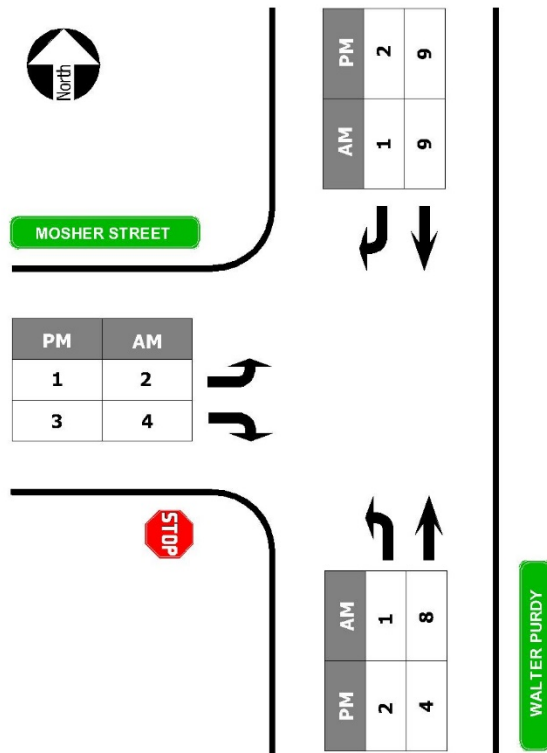


Exhibit 3.5 – Willow Street at Walter Purdy Avenue Estimated Total Traffic 2027

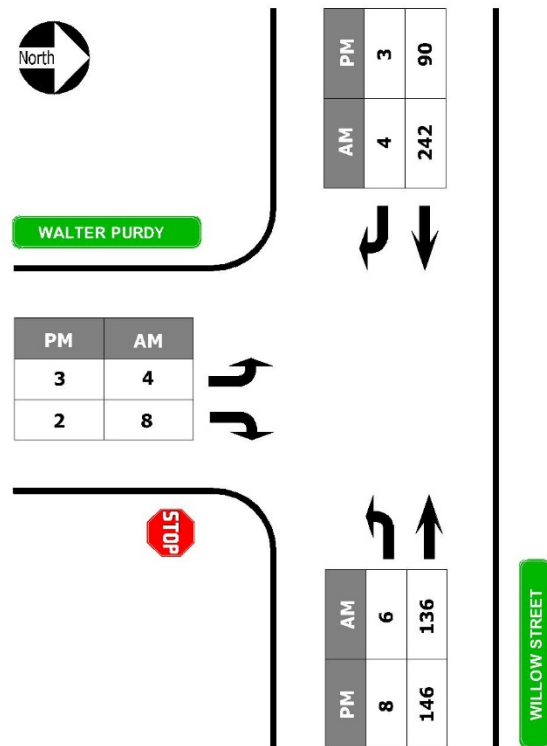
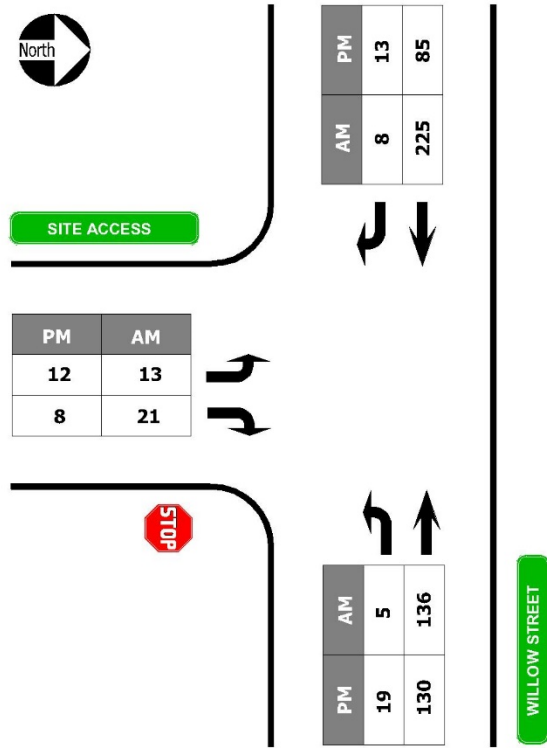


Exhibit 3.5 – Site Access (Gallagher Street) at Willow Street Estimated Total Traffic 2027



## 4 Evaluation of Impacts

### 4.1 Level of Service Analysis

As described in the Highway Capacity Manual *"the concept of levels of service used qualitative measures that characterize operational conditions within a traffic stream and their perception by motorists and passengers. The descriptions of individual levels of service characterize these conditions in terms of such factors as speed and travel time, freedom to maneuver, traffic interruptions, and comfort and convenience."*

*Six levels of service are defined for each type of facility for which analysis procedures are available. They are given letter designations from A to F, with LOS A representing the best operating conditions and LOS F the worst."*

As stated in the Highway Capacity Manual, *"analysis of signalized intersections focuses on the capacity and level of service of intersection approaches and the intersection as a whole. Capacity is evaluated in terms of the ratio of demand flow rate (volume) to capacity (v/c ratio) while the level of service is evaluated on the basis of average control delay per vehicle (in seconds per vehicle)." Exhibit 4.1 defines Level of Service for signalized intersections.*

The Highway Capacity Manual also states that *"the level of service is determined by the computed or measured control delay and is defined for each minor movement. Level of Service is not defined for the intersection as a whole."* LOS criteria for unsignalized intersections are summarized in Exhibit 4.2.

Exhibit 4.1 - Level of Service Criteria for Signalized Intersections

Level of Service	Description	Control, Delay Per Vehicle (Seconds)
A	Very low delay; most vehicles do not stop (Excellent)	≤ 10
B	Higher delay; more vehicles stop (Very Good)	≥ 10 and ≤ 20
C	Higher number of congestion; number of vehicles stopping is significant, although many still pass through intersection without stopping (Good)	≥ 20 and ≤ 35
D	Congestion becomes noticeable; vehicles must sometimes wait through more than one red light; Many vehicles stop (Satisfactory)	≥ 35 and ≤ 55
E	Vehicles must often wait through more than one red light; considered by many agencies to be the limit of acceptable delay	≥ 55 and ≤ 80
F	This level is considered to be unacceptable for most drivers; occurs when arrival flow rates exceed the capacity of the intersection (Unacceptable)	≥ 80

Exhibit 4.2 - Level of Service Criteria for Unsignalized Intersections

Level of Service	Delay Range (Seconds)
A	≤ 10
B	≥ 10 and ≤ 15
C	≥ 15 and ≤ 25
D	≥ 25 and ≤ 35
E	≥ 35 and ≤ 50
F	≥ 50

Traffic volumes are at their highest during the AM and PM peak periods so the impact of the trips generated by the proposed development during these hours will provide a worst case assessment of their impacts on the existing transportation network.

NSTIR’s Guide for the Preparation of Traffic Impact Studies states that:

*"for signalized and unsignalized intersections and overall LOS rating of "A" to "D" (based on delay), and a volume/capacity (v/c) ratio less than 0.90 for any individual movement, are normally considered acceptable. Where existing or horizon year levels of service are "E" or "F", or v/c ratios exceed 0.90, without the proposed development, LOS and v/c ratios equal to or better than existing levels may be acceptable as long as the average stopped delay per vehicle per movement is not increased after the development."*

Level of Service (LOS), Volume-to-Capacity ratios (v/c) and 95% Queue Length in vehicles (95%) results from all key movements at all intersections in the study area are provided in the following Exhibits and detailed Synchro reports have been included in the Appendix.

Exhibit 4.3 – Donald Street at Walter Purdy Avenue Level of Service Results

	Donald Street		Walter Purdy Avenue		Total
	EB-L	EB-T	SB-LR		
<b>AM PEAK HOUR – EXISTING TRAFFIC 2022</b>					
Delay	7.4	-	9.2		1.2
LOS	A	A	A		
v/c	0.005		0.018		
95% Queue	0.0		0.1		
<b>AM PEAK HOUR – BACKGROUND TRAFFIC 2027</b>					
Delay	7.5	-	9.2		1.2
LOS	A	A	A		
v/c	0.006		0.02		
95% Queue	0.0		0.1		
<b>AM PEAK HOUR – TOTAL TRAFFIC 2027</b>					
Delay	7.5	-	9.4		1.9
LOS	A	A	A		
v/c	0.007		0.039		
95% Queue	0.0		0.1		
<b>PM PEAK HOUR – EXISTING TRAFFIC 2022</b>					
Delay	7.4	-	8.8		1.2
LOS	A	A	A		
v/c	0.003		0.005		
95% Queue	0.0		0.0		
<b>PM PEAK HOUR – BACKGROUND TRAFFIC 2027</b>					
Delay	7.3	-	8.8		1.2
LOS	A	A	A		
v/c	0.003		0.005		
95% Queue	0.0		0.0		
<b>PM PEAK HOUR – TOTAL TRAFFIC 2027</b>					
Delay	7.3	-	8.9		2.2
LOS	A	A	A		
v/c	0.006		0.011		
95% Queue	0.0		0.0		

All key intersection movements will operate with acceptable LOS during the AM and PM peak with existing traffic in 2022 and will continue to do so with the addition of site generated traffic and background traffic estimates in 2027.

Exhibit 4.4 – Brown Street at Walter Purdy Avenue Level of Service Results

	Brown Street		Walter Purdy Avenue		Total
	EB-LR		NB-L	NB-T	
<b>AM PEAK HOUR – EXISTING TRAFFIC 2022</b>					
Delay	8.7		7.3	-	1.4
LOS	A		A	A	
v/c	0.003		0.001		
95% Queue	0.0		0.0		
<b>AM PEAK HOUR – BACKGROUND TRAFFIC 2027</b>					
Delay	8.7		7.3	-	1.3
LOS	A		A	A	
v/c	0.003		0.001		
95% Queue	0.0		0.0		
<b>AM PEAK HOUR – TOTAL TRAFFIC 2027</b>					
Delay	8.6		7.3	-	3.7
LOS	A		A	A	
v/c	0.014		0.001		
95% Queue	0.0		0.0		
<b>PM PEAK HOUR – EXISTING TRAFFIC 2022</b>					
Delay	8.7		7.3	-	1.2
LOS	A		A	A	
v/c	0.001		0.004		
95% Queue	0.0		0.0		
<b>PM PEAK HOUR – BACKGROUND TRAFFIC 2027</b>					
Delay	8.8		7.3	-	1.1
LOS	A		A	A	
v/c	0.001		0.001		
95% Queue	0.0		0.0		
<b>PM PEAK HOUR – TOTAL TRAFFIC 2027</b>					
Delay	8.6		7.3	-	3.6
LOS	A		A	A	
v/c	0.005		0.005		
95% Queue	0.0		0.0		

All key intersection movements will operate with acceptable LOS during the AM and PM peak with existing traffic in 2022 and will continue to do so with the addition of site generated traffic and background traffic estimates in 2027.

Exhibit 4.4 – Mosher Street at Walter Purdy Avenue Level of Service Results

	Mosher Street		Walter Purdy Avenue		Total
	EB-LR		NB-L	NB-T	
<b>AM PEAK HOUR – EXISTING TRAFFIC 2022</b>					
Delay	8.7		7.3	-	1.4
LOS	A		A	A	
v/c	0.005		-		
95% Queue	0.0		0.0		
<b>AM PEAK HOUR – BACKGROUND TRAFFIC 2027</b>					
Delay	8.7		7.3	-	1.2
LOS	A		A	A	
v/c	0.005		-		
95% Queue	0.0		0.0		
<b>AM PEAK HOUR – TOTAL TRAFFIC 2027</b>					
Delay	8.7		7.3	-	2.4
LOS	A		A	A	
v/c	0.009		0.001		
95% Queue	0.0		0.0		
<b>PM PEAK HOUR – EXISTING TRAFFIC 2022</b>					
Delay	8.7		7.3	-	1.2
LOS	A		A	A	
v/c	0.002		-		
95% Queue	0.0		0.0		
<b>PM PEAK HOUR – BACKGROUND TRAFFIC 2027</b>					
Delay	8.7		7.3	-	1.0
LOS	A		A	A	
v/c	0.002		-		
95% Queue	0.0		0.0		
<b>PM PEAK HOUR – TOTAL TRAFFIC 2027</b>					
Delay	8.6		7.3	-	2.3
LOS	A		A	A	
v/c	0.005		0.002		
95% Queue	0.0		0.0		

All key intersection movements will operate with acceptable LOS during the AM and PM peak with existing traffic in 2022 and will continue to do so with the addition of site generated traffic and background traffic estimates in 2027.

Exhibit 4.5 – Willow Street at Walter Purdy Avenue Level of Service Results

	Willow Street		Walter Purdy Avenue		Total
	WB-L	WB-T	NB-LR		
<b>AM PEAK HOUR – EXISTING TRAFFIC 2022</b>					
Delay	8.0	-	11.1		0.5
LOS	A	A	B		
v/c	0.006		0.028		
95% Queue	0.0		0.1		
<b>AM PEAK HOUR – BACKGROUND TRAFFIC 2027</b>					
Delay	8.1	-	11.4		0.5
LOS	A	A	B		
v/c	0.008		0.032		
95% Queue	0.0		0.1		
<b>AM PEAK HOUR – TOTAL TRAFFIC 2027</b>					
Delay	8.1	-	11.8		0.5
LOS	A	A	B		
v/c	0.008		0.034		
95% Queue	0.0		0.1		
<b>PM PEAK HOUR – EXISTING TRAFFIC 2022</b>					
Delay	7.5	-	9.6		0.5
LOS	A	A	A		
v/c	0.006		0.008		
95% Queue	0.0		0.0		
<b>PM PEAK HOUR – BACKGROUND TRAFFIC 2027</b>					
Delay	7.5	-	9.8		0.5
LOS	A	A	A		
v/c	0.007		0.008		
95% Queue	0.0		0.0		
<b>PM PEAK HOUR – TOTAL TRAFFIC 2027</b>					
Delay	7.5	-	9.9		0.4
LOS	A	A	A		
v/c	0.007		0.008		
95% Queue	0.0		0.0		

All key intersection movements will operate with acceptable LOS during the AM and PM peak with existing traffic in 2022 and will continue to do so with the addition of site generated traffic and background traffic estimates in 2027.

Exhibit 4.6 – Site Access (Gallagher Street) at Willow Street Level of Service Results

	Willow Street		Site Access		Total
	WB-L	WB-T	NB-LR		
<b>AM PEAK HOUR – TOTAL TRAFFIC 2027</b>					
Delay	8.1	-	12.1		1.1
LOS	A	A	B		
v/c	0.007		0.095		
95% Queue	0.0		0.3		
<b>PM PEAK HOUR – TOTAL TRAFFIC 2027</b>					
Delay	7.5	-	10.1		1.3
LOS	A	A	B		
v/c	0.016		0.033		
95% Queue	0.0		0.1		

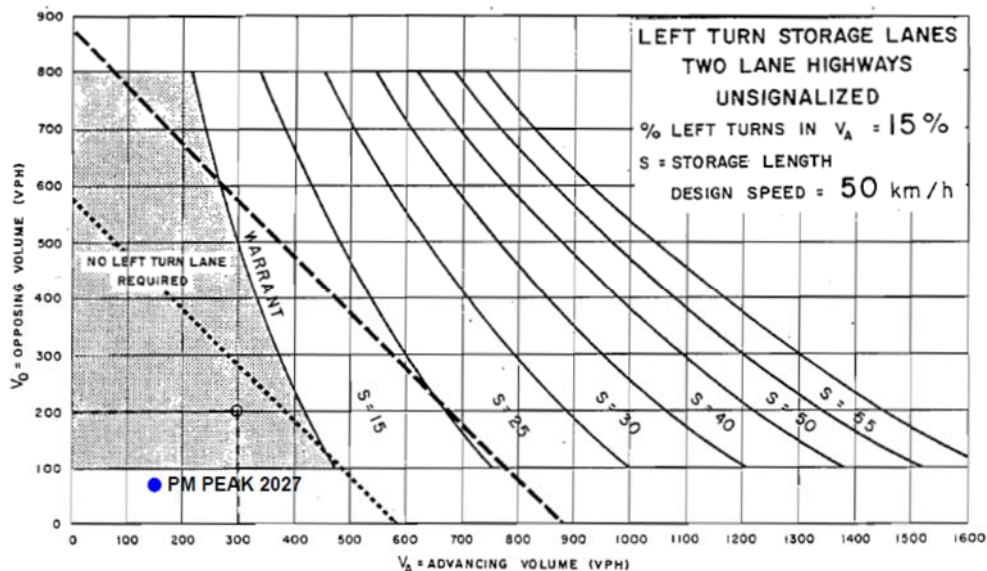
All key intersection movements will operate with acceptable LOS during the AM and PM peak with site generated traffic at this proposed connection to Willow Street.

#### 4.2 Turning Lane Assessments

Left turn warrants and storage lane lengths for unsignalized intersections are based on turning, advancing and opposing design hour volumes and to determine if turning traffic will impede through traffic to a level that will warrant a dedicated left turn lane.

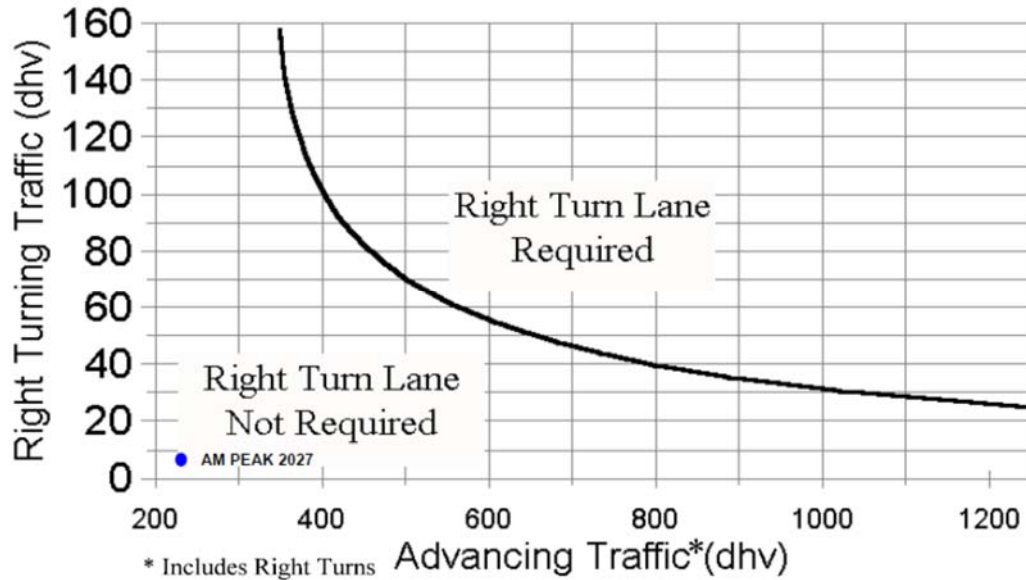
Ministry of Transportation for the Province of Ontario (MTO) procedures to determine the requirement for left turn lanes provide graphs that assess left turn volumes versus Opposing Volume for four-lane undivided highways. Westbound left turn traffic volumes are higher in the PM peak hour at the proposed site access (19 vehicles) and a dedicated left turn lane is not warranted as indicated in Exhibit 4.5.

Exhibit 4.5 – Left Turn Warrant on Willow Street at Site Access (Gallagher Extension)



Eastbound right turn traffic entering the development is also very low at 8 vehicles in the AM peak hour and as a result a right turn lane is not warranted based on procedures and graphs prepared by the Ohio Department of Transportation. Refer to Exhibit 4.6.

Exhibit 4.6 – Right Turn Warrant on Reeves Street at Site Access Driveway



### 4.3 Stopping Site Distance

As per the Transportation of Canada Geometric Design Guide for Canadian Roads, adequate stopping site distance *"is essential for safe operation that the vehicle operator be able to see far enough ahead to stop if necessary. Conditions that would force a vehicle operator to stop are for example, an object on the roadway, a culvert washout or other fault in the roadway.*

*Adequate stopping site distance is required throughout the length of the roadway. Minimum stopping site distance is the sum of two distances namely:*

- *Brake reaction distance*

*The distance travelled during the brake reaction time, that is the time that elapses from the instant an object, for which the driver decides to stop, comes into view to the instant the driver takes remedial action (contacts brake pedal).*

- *Braking distance*

*The distance travelled from the time that braking begins to the time the vehicle comes to a stop."*

For a design speed of 70 km/h, the minimum stopping site distance is 110 m.

We completed a site review and didn't identify any red flags or concerns providing adequate stopping site distance on Willow Street at the proposed development as visibility is good at the site to the east and west as shown in site photos in Section 2.2. We did not complete a formal stopping site distance survey.

#### 4.4 Site Access

The site plan provided for the proposed development shows three new 54-unit apartment buildings for a total of 162 units with primary access from an extension of the existing Gallagher Street to Willow Street. A driveway to the property will be established from this extension and a second access point will be created at the western end of Gallagher Street at Brown Street. A new internal road will be constructed on the property to provide access to the apartment buildings and three surface parking areas that will contain 204 parking spaces. An emergency vehicle turnaround is included on site.

#### 4.5 Surrounding Street Network

Our manual traffic counts can help estimate 24-hour Average Annual Daily Traffic Volumes on the streets surrounding the proposed development. We reviewed 24-hour hourly counts (Average Annual Daily Traffic Volumes) provided by NSTIR that were completed in the area of the proposed development in 2019 on NS Highway 6 just west of Route 366. We assessed the AM and PM peak hours from these counts as we determined that these peak hours account for 14% of the total AADT volumes.

We applied this ratio to our observed manual traffic counts completed on June 20<sup>th</sup> to estimate AADT volumes on the key roads surrounding the proposed Lot 2021-2R development as summarized below in Exhibit 4.7.

Exhibit 4.7 – Estimates AADT Volumes Surrounding Proposed Lot 2021-2R Development

Street	2022 EXISTING TRAFFIC			2027 TOTAL TRAFFIC		
	AM PEAK	PM PEAK	AADT	AM PEAK	PM PEAK	AADT
Willow Street	339	204	3833	400	253	4608
Walter Purdy Avenue	37	23	424	55	34	624
Donald Street	108	48	1101	133	65	1395
Site Access (Gallagher Street)				46	33	560

Willow Street is a designated arterial street and existing and estimated future volumes with the proposed development are well under TAC’s guidelines for a residential minor arterial road of between 5,000 and 20,000 vehicles/day.

Estimated existing daily traffic volumes on Donald Street are relatively low and just exceed TACs guidelines for a local residential road at 1,000 vehicles per day while being well under TAC guidelines for a residential collector road of less than 8,000 vehicles per day. We also note that HRM guidelines for a local road are up to 3,000 vehicles per day. The impact of the proposed development plus background traffic only marginally increase daily traffic on Donald Street.

Estimated existing daily and future traffic volumes on Walter Purdy Avenue (and by extension on Brown Street and Mosher Street) are well below TAC guidelines for local roads. Future estimated daily traffic volumes on the proposed Gallagher Street extension to Willow Street are also below TAC’s guidelines for a local road.

## 4.6 Pedestrians

As highlighted in Section 2.5 the pedestrian network in the area has been well established with sidewalks on the north sides of Willow Street and Donald Street to provide safe access for walking. Marked crosswalks are installed on Willow Street at the bus entrance to EB Chandler Junior High School and at Townshend Avenue. Crosswalks are also installed at the entrance to EB Chandler Junior High School on Donald Street.

Willow Street was upgraded to include bike lanes on the both sides contributing to a pedestrian and bicycle friendly area which helps residents bike, walk and use other human power ways to move around the city.

The site plan for the proposed development shows a sidewalk network on site to connect the building and parking areas. We recommend that the extension of Gallagher Street include a sidewalk on its eastern side to connect the pedestrian facilities on site to the existing sidewalk on the northern side of Willow Street.

We reviewed Transportation Association of Canada's Pedestrian Crossing Control Guide which references a holistic approach where a *"complete street is one that adequately provides all road users including bicyclists, pedestrians, transit riders, and motorists, to the extent appropriate to the function and context of the street. Knowing and understanding the desire lines (i.e the walking paths that pedestrians will choose from an origin to a selected destination) that are typically created by land use type and location, roadway cross section and geometry, and traffic control devices, can lease to the effective and safer management of vehicular and pedestrian traffic. The complimentary relationship among transportation, land use planning and urban design creates walkable and healthier communities, with road systems that support walking as an important part of people's daily lives."*

This guide recommends that a pedestrian control device be considered for an average hourly pedestrian volume of 15 Equivalent Adult Units (EAUs). We note that adults are equivalent to 1.0 EAU's, unaccompanied children  $\leq 12$  years old are 2.0 EAU's, Seniors  $\geq 65$  years year are 1.5 EAU's and pedestrians with physical impairments are equivalent to 2.0 EAU's. This threshold is based on a minimum volume of 100 pedestrians over a 7-hour continuous counting period.

The minimum practical traffic volumes at which the installation of a pedestrian crossing control device should be considered is 1,500 vehicles per day. Additional consideration is given to proximity to other traffic control devices. Close proximity is any distance between 100 and 200 meters. The marked crosswalk at EB Chandler Junior High School is located approximately 295 meters from the proposed site access on Willow Street and the existing marked crosswalk at Townshend Avenue is located approximately 225 meters away.

After consideration of TAC's guidelines and the area we recommend that a pedestrian control device (marked crosswalk) be installed on Willow Street to connect the existing sidewalk to the proposed sidewalk on the site access (Gallagher Street Extension). A crosswalk with side mounted signs in accordance with TAC guidelines and Town of Amherst standards is appropriate here to provide a safe opportunity for pedestrians to cross Willow Street.

## 5 Conclusions and Recommendations

- This Traffic Impact Study has provided a detailed assessment of the potential traffic impacts of the proposed development of three 54 unit apartment buildings at Lot 2021-2R in Amherst, Nova Scotia.
- We estimate that the proposed development will generate **60** new vehicle trips (16 enter, 44 exit) in the AM Peak Hour **64** new vehicle trips (39 enter, 25 exit) in the PM Peak Hour and **726** vehicles on a weekday.
- New site generated traffic will most likely follow existing trip distribution patterns in the surrounding area with 77% of site generated traffic using Willow Street for access/egress in the AM peak hour with the balance of 23% using Donald Street/Walter Purdy Avenue. In the PM peak hour 82% of site generated traffic will use Willow Street for access/egress with 18% using Donald Street/Walter Purdy Avenue
- We recommend that the extension of Gallagher Street to Willow Street incorporates a sidewalk on its eastern side to connect the pedestrian facilities on site to the existing sidewalk on the northern side of Willow Street.
- We recommend that a pedestrian control device (marked crosswalk) be installed on Willow Street to connect the existing sidewalk on its northern side to the proposed sidewalk on the Gallagher Street Extension. A crosswalk with side mounted signs in accordance with Transportation Association of Canada and Town of Amherst Design Guidelines is appropriate
- New roads and sidewalks should be constructed in accordance with Transportation Association of Canada and Town of Amherst Design Guidelines.
- We recommend that the site plan and the proposed internal road network be reviewed to ensure that emergency vehicles and larger service vehicles can be accommodated. This internal road network should be designed to Transportation Association of Canada and Town of Amherst Design Guidelines
- A new stop sign will be required at the northern end of the proposed Gallagher Street Extension facing Willow Street. We also recommend that stop signs be installed at the eastern ends of Brown Street and Mosher Street facing Gallagher Street. All new signs and pavement markings should be installed in accordance with TAC's Manual of Uniform Traffic Control Devices for Canada (MUTCDC).
- A visual review of stopping site distance at the site didn't identify any concerns with providing adequate stopping site distance at the proposed site access from Willow Street (Gallagher Street extension) but we do recommend that vegetation be monitored and removed from the right-of-way to the west and east of the property to ensure the decision sight triangle is maintained for exiting vehicles.
- Traffic created by this proposed residential development can be introduced safely and efficiently into the existing transportation network in Amherst, Nova Scotia with the recommendations outlined in this section.

## APPENDIX

TRAFFIC COUNTS

TRIP GENERATION ESTIMATES

TOTAL TRAFFIC ANALYSIS

SYNCHRO 10 REPORTS

# LOT 2021-2R AMHERST TRAFFIC IMPACT STUDY

## MANUAL TRAFFIC COUNTS

PEAK HOUR TRAFFIC COUNTS COMPLETED ON MONDAY JUNE 20, 2022

<b>AM PEAK</b>		<b>WALTER PURDY AVENUE</b>						<b>DONALD STREET</b>					
ENTER		SOUTHBOUND						EASTBOUND			WESTBOUND		
EXIT		L	T	R	L	T	R	L	T	R	L	T	R
<b>2022</b>													
07:00:00 AM	07:15:00 AM	1		1				1	2			0	0
07:15:00 AM	07:30:00 AM	1		1				0	3			3	0
07:30:00 AM	07:45:00 AM	2		1				0	3			5	0
07:45:00 AM	08:00:00 AM	1		1				0	4			3	0
08:00:00 AM	08:15:00 AM	2		1				0	2			4	1
08:15:00 AM	08:30:00 AM	1		2				0	8			13	0
08:30:00 AM	08:45:00 AM	0		1				4	20			27	1
08:45:00 AM	09:00:00 AM	1		2				1	9			8	0
<b>2022</b>													
08:00:00 AM	09:00:00 AM	4		6				5	39			52	2

<b>PM PEAK</b>		<b>WALTER PURDY AVENUE</b>						<b>DONALD STREET</b>					
ENTER	3	NORTHBOUND						EASTBOUND			WESTBOUND		
EXIT	6	L	T	R	L	T	R	L	T	R	L	T	R
<b>2022</b>													
04:00:00 PM	04:15:00 PM	0		2				1	5			5	0
04:15:00 PM	04:30:00 PM	1		1				1	6			4	0
04:30:00 PM	04:45:00 PM	1		0				2	6			7	0
04:45:00 PM	05:00:00 PM	0		0				1	3			1	0
05:00:00 PM	05:15:00 PM	0		1				0	5			8	0
05:15:00 PM	05:30:00 PM	0		0				1	1			6	0
05:30:00 PM	05:45:00 PM	0		2				2	1			3	0
05:45:00 PM	06:00:00 PM	0		2				0	1			2	0
<b>2022</b>													
04:15:00 PM	05:15:00 PM	2		2				4	20	0	0	20	0

# LOT 2021-2R AMHERST TRAFFIC IMPACT STUDY

## MANUAL TRAFFIC COUNTS

PEAK HOUR TRAFFIC COUNTS COMPLETED ON MONDAY JUNE 20, 2022

AM PEAK		WALTER PURDY AVENUE						WILLOW STREET					
ENTER		NORTHBOUND						EASTBOUND			WESTBOUND		
EXIT		L	T	R	L	T	R	L	T	R	L	T	R
<b>2022</b>													
07:00:00 AM	07:15:00 AM	0		1					9	1	0	7	
07:15:00 AM	07:30:00 AM	0		0					17	0	0	5	
07:30:00 AM	07:45:00 AM	0		1					29	0	0	6	
07:45:00 AM	08:00:00 AM	1		0					34	0	0	6	
08:00:00 AM	08:15:00 AM	2		3					38	0	1	12	
08:15:00 AM	08:30:00 AM	0		0					61	0	2	36	
08:30:00 AM	08:45:00 AM	1		3					75	2	2	49	
08:45:00 AM	09:00:00 AM	1		1					26	2	0	22	
<b>2022</b>													
08:00:00 AM	09:00:00 AM	4		7					200	4	5	119	

PM PEAK		WALTER PURDY AVENUE						WILLOW STREET					
ENTER	3	NORTHBOUND						EASTBOUND			WESTBOUND		
EXIT	6	L	T	R	L	T	R	L	T	R	L	T	R
<b>2022</b>													
04:00:00 PM	04:15:00 PM	2		0					21	1	0	26	
04:15:00 PM	04:30:00 PM	0		0					18	0	3	35	
04:30:00 PM	04:45:00 PM	1		0					12	1	3	20	
04:45:00 PM	05:00:00 PM	0		1					17	0	1	31	
05:00:00 PM	05:15:00 PM	2		1					27	2	0	29	
05:15:00 PM	05:30:00 PM	0		0					16	0	1	34	
05:30:00 PM	05:45:00 PM	0		1					17	0	2	18	
05:45:00 PM	06:00:00 PM	0		2					19	0	0	28	
<b>2022</b>													
04:15:00 PM	05:15:00 PM	3		2					74	3	7	115	

# LOT 2021-2R AMHERST TRAFFIC IMPACT STUDY

## TRIP GENERATION ESTIMATES

Source - ITE Trip Generation Manual 11th Edition

**Land Use 221** Multi Family Housing (Mid-Rise)  
**AM PEAK**  $T = 0.44(X) - 11.61$   
**PM PEAK**  $T = 0.39(X) + 0.34$   
**WEEKDAY**  $T = 4.77(X) - 46.46$

**Land Use 221** Multi Family Housing (Mid-Rise)  
**AM PEAK** 0.37 Average Rate  
**PM PEAK** 0.39 Average Rate  
**WEEKDAY** 4.54 Average Rate

**Land Use 210** Single Family Detached Housing  
**AM PEAK**  $\ln(T) = 0.91\ln(X) + 0.12$   
**PM PEAK**  $\ln(T) = 0.94\ln(X) + 0.27$   
**WEEKDAY**  $\ln(T) = 0.92\ln(X) + 2.68$

**Land Use 210** Single Family Detached Housing  
**AM PEAK** 0.7 Average Rate  
**PM PEAK** 0.94 Average Rate  
**WEEKDAY** 9.43 Average Rate

PROPOSED APARTMENTS										
LAND USE	QUANTITY	AM PEAK			PM PEAK			WEEKDAY		
		TOTAL TRIPS	ENTER	EXIT	TOTAL	ENTER	EXIT	TOTAL	ENTER	EXIT
Apartment Mid Rise	162	60	26%	74%	64	61%	39%	726	50%	50%
			16	44		39	25		363	363
<b>TOTAL</b>		<b>60</b>	<b>16</b>	<b>44</b>	<b>64</b>	<b>39</b>	<b>25</b>	<b>726</b>	<b>363</b>	<b>363</b>

\* Fitted Curve Equations were used to estimate vehicle trips

EXISTING SINGLE FAMILY										
LAND USE	QUANTITY	AM PEAK			PM PEAK			WEEKDAY		
		TOTAL TRIPS	ENTER	EXIT	TOTAL	ENTER	EXIT	TOTAL	ENTER	EXIT
Single Family	21	18	26%	74%	23	63%	37%	240	50%	50%
			5	13		14	8		120	120
<b>TOTAL</b>		<b>18</b>	<b>5</b>	<b>13</b>	<b>23</b>	<b>14</b>	<b>8</b>	<b>240</b>	<b>120</b>	<b>120</b>

\* Existing Single Family Homes located between Willow Street and Donald Street on Walter Purdy Avenue, Mosher Street, Gallagher Street and Brown Street

\* Fitted Curve Equations were used to estimate vehicle trips

# Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

**Vehicle Trip Ends vs: Dwelling Units**  
**On a: Weekday,**  
**Peak Hour of Adjacent Street Traffic,**  
**One Hour Between 7 and 9 a.m.**

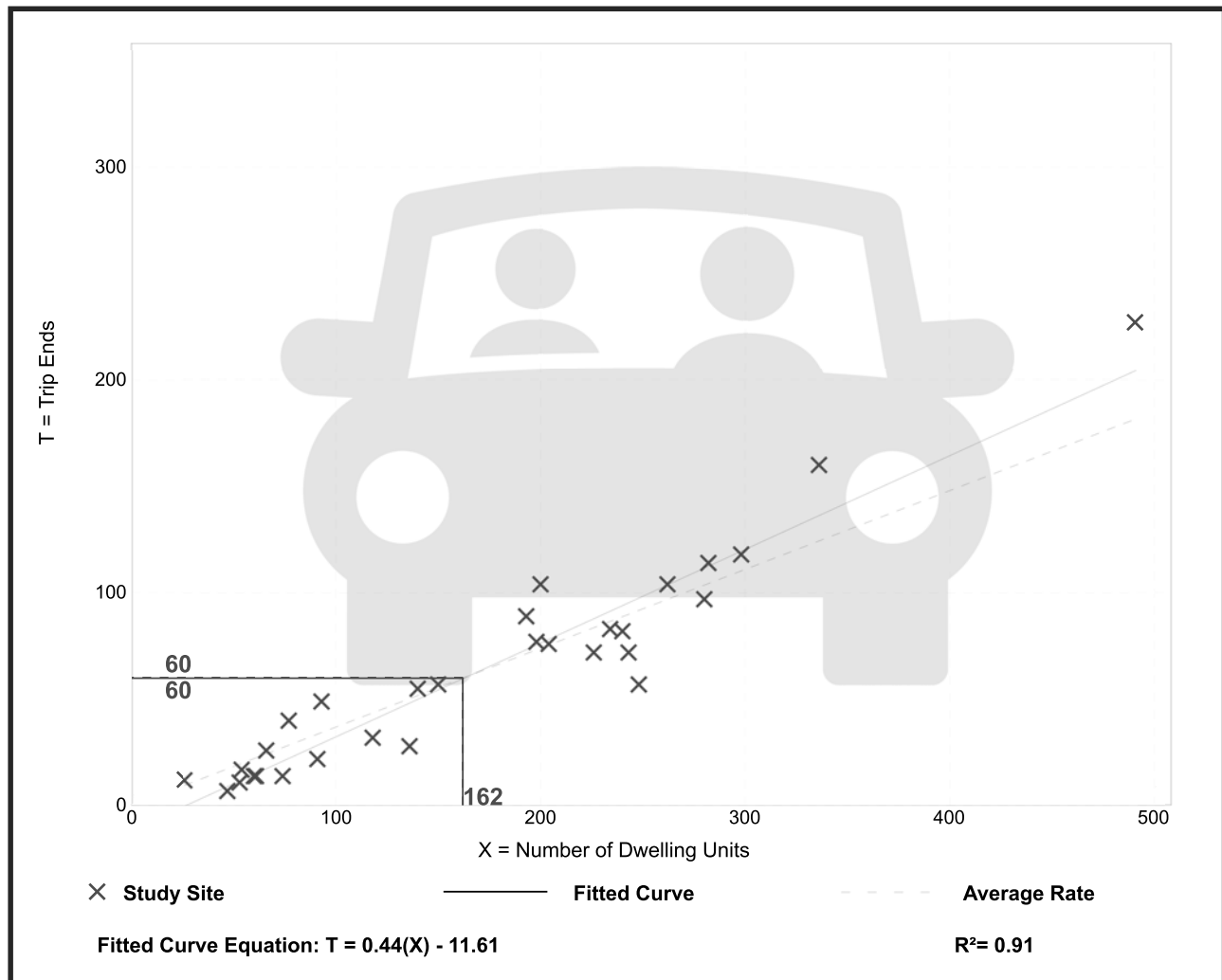
**Setting/Location: General Urban/Suburban**

Number of Studies: 30  
 Avg. Num. of Dwelling Units: 173  
 Directional Distribution: 23% entering, 77% exiting

## Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.37	0.15 - 0.53	0.09

## Data Plot and Equation



# Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

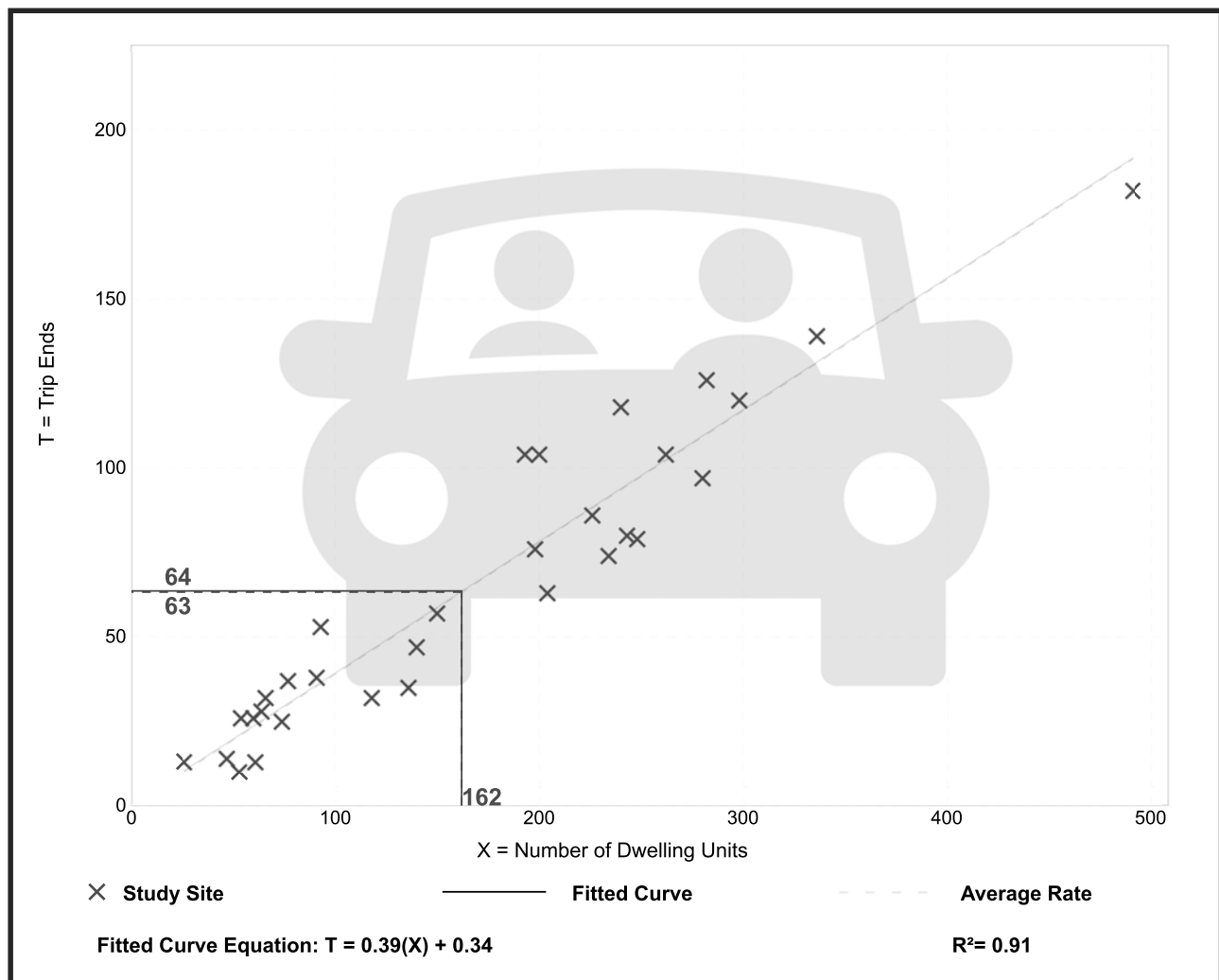
**Vehicle Trip Ends vs: Dwelling Units**  
**On a: Weekday,**  
**Peak Hour of Adjacent Street Traffic,**  
**One Hour Between 4 and 6 p.m.**

**Setting/Location: General Urban/Suburban**  
 Number of Studies: 31  
 Avg. Num. of Dwelling Units: 169  
 Directional Distribution: 61% entering, 39% exiting

## Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.39	0.19 - 0.57	0.08

## Data Plot and Equation



# Multifamily Housing (Mid-Rise) Not Close to Rail Transit (221)

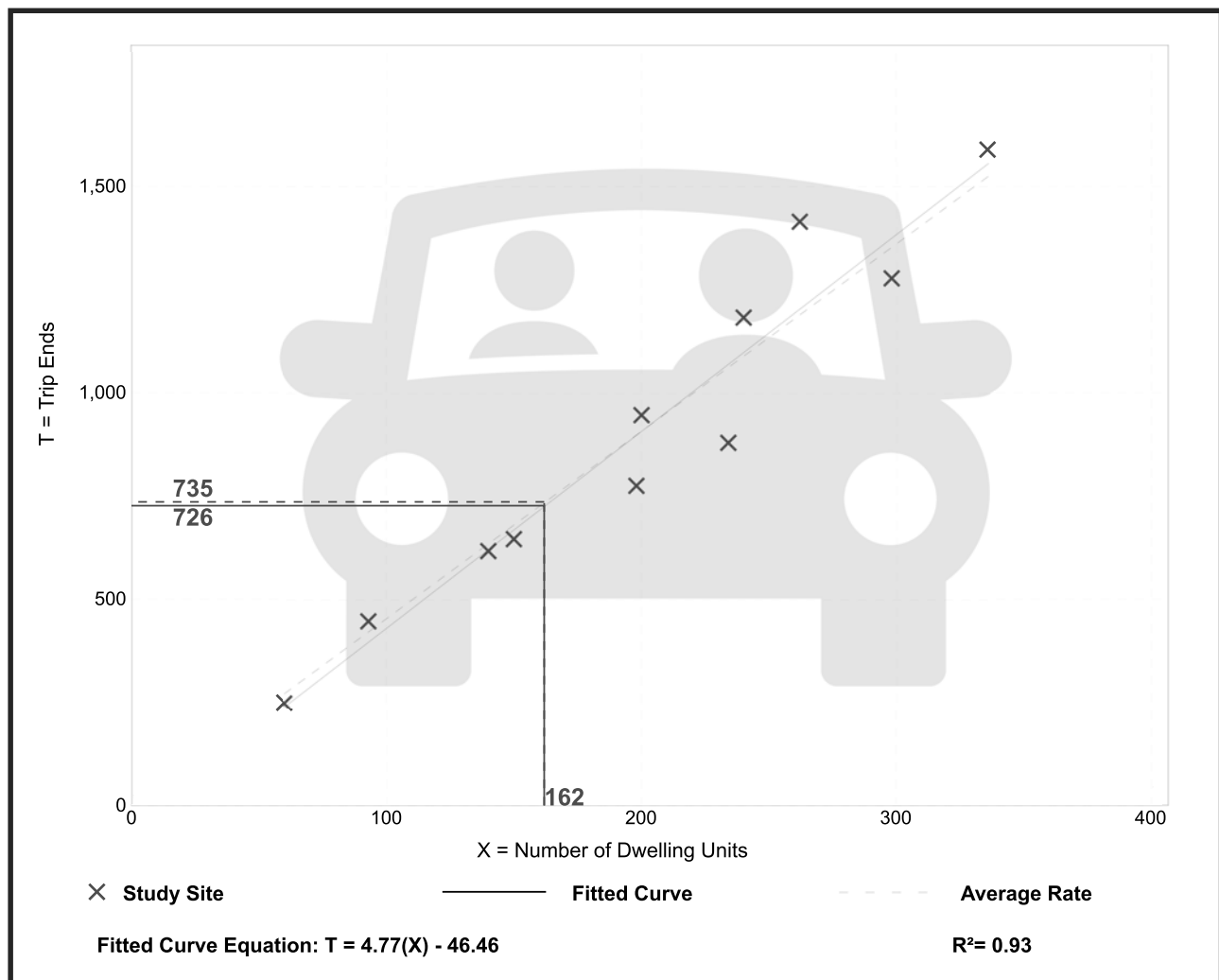
Vehicle Trip Ends vs: Dwelling Units  
On a: Weekday

Setting/Location: General Urban/Suburban  
Number of Studies: 11  
Avg. Num. of Dwelling Units: 201  
Directional Distribution: 50% entering, 50% exiting

## Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
4.54	3.76 - 5.40	0.51

## Data Plot and Equation



# LOT 2021-2R AMHERST TRAFFIC IMPACT STUDY

## TOTAL TRAFFIC ANALYSIS

PEAK HOUR TRAFFIC COUNTS COMPLETED ON MONDAY JUNE 20, 2022

AM PEAK		WALTER PURDY AVENUE						DONALD STREET					
ENTER	16	SOUTHBOUND						WESTBOUND			EASTBOUND		
EXIT	44	L	T	R	L	T	R	L	T	R	L	T	R

2022													
08:00:00 AM	08:15:00 AM	2		1					4	1	0	2	
08:15:00 AM	08:30:00 AM	1		2					13	0	0	8	
08:30:00 AM	08:45:00 AM	0		1					27	1	4	20	
08:45:00 AM	09:00:00 AM	1		2					8	0	1	9	
2022													
08:00:00 AM	09:00:00 AM	4		6					52	2	5	39	
2027													
08:00:00 AM	09:00:00 AM	4		7					57	2	6	43	
DISTRIBUTION													
08:00:00 AM	09:00:00 AM								55%			45%	
SITE GENERATED TRAFFIC													
08:00:00 AM	09:00:00 AM	5		6						2	2		
TOTAL TRAFFIC 2027													
08:00:00 AM	09:00:00 AM	9		12					57	4	7	43	

PM PEAK		WALTER PURDY AVENUE						DONALD STREET					
ENTER	39	SOUTHBOUND						WESTBOUND			EASTBOUND		
EXIT	25	L	T	R	L	T	R	L	T	R	L	T	R

2022													
04:15:00 PM	04:30:00 PM	1		1					4	0	1	6	
04:30:00 PM	04:45:00 PM	1		0					7	0	2	6	
04:45:00 PM	05:00:00 PM	0		0					1	0	1	3	
05:00:00 PM	05:15:00 PM	0		1					8	0	0	5	
2022													
04:15:00 PM	05:15:00 PM	2		2					20	0	4	20	
2027													
04:15:00 PM	05:15:00 PM	2		2					22	0	4	22	
DISTRIBUTION													
04:15:00 PM	05:15:00 PM								45%			55%	
SITE GENERATED TRAFFIC													
04:15:00 PM	05:15:00 PM	2		2						3	4		
TOTAL TRAFFIC 2027													
04:15:00 PM	05:15:00 PM	5		4					22	3	8	22	

# LOT 2021-2R AMHERST TRAFFIC IMPACT STUDY

## TOTAL TRAFFIC ANALYSIS

PEAK HOUR TRAFFIC COUNTS COMPLETED ON MONDAY JUNE 20, 2022

AM PEAK		WALTER PURDY AVENUE						BROWN STREET					
ENTER		SOUTHBOUND			NORTHBOUND			WESTBOUND			EASTBOUND		
EXIT		L	T	R	L	T	R	L	T	R	L	T	R
<b>2022</b>													
08:00:00 AM	08:15:00 AM												
08:15:00 AM	08:30:00 AM												
08:30:00 AM	08:45:00 AM												
08:45:00 AM	09:00:00 AM												
<b>2022</b>			8	0	1	6					1		1
<b>2027</b>			9	0	1	7					1		1
<b>DISTRIBUTION</b>													
08:00:00 AM	09:00:00 AM												
<b>SITE GENERATED TRAFFIC</b>					2								7
<b>TOTAL TRAFFIC 2027</b>			9	0	4	7					1		8
08:00:00 AM	09:00:00 AM		9	0	4	7					1		8

PM PEAK		WALTER PURDY AVENUE						BROWN STREET					
ENTER		SOUTHBOUND			NORTHBOUND			WESTBOUND			EASTBOUND		
EXIT		L	T	R	L	T	R	L	T	R	L	T	R
<b>2022</b>													
04:15:00 PM	04:30:00 PM												
04:30:00 PM	04:45:00 PM												
04:45:00 PM	05:00:00 PM												
05:00:00 PM	05:15:00 PM												
<b>2022</b>			6	2	1	3					1		0
<b>2027</b>			7	2	1	3					1		0
<b>DISTRIBUTION</b>													
04:15:00 PM	05:15:00 PM												
<b>SITE GENERATED TRAFFIC</b>					5								3
<b>TOTAL TRAFFIC 2027</b>			7	2	6	3					1		3
04:15:00 PM	05:15:00 PM		7	2	6	3					1		3

# LOT 2021-2R AMHERST TRAFFIC IMPACT STUDY

## TOTAL TRAFFIC ANALYSIS

PEAK HOUR TRAFFIC COUNTS COMPLETED ON MONDAY JUNE 20, 2022

AM PEAK		WALTER PURDY AVENUE						MOSHER STREET					
ENTER		SOUTHBOUND			NORTHBOUND			WESTBOUND			EASTBOUND		
EXIT		L	T	R	L	T	R	L	T	R	L	T	R
<b>2022</b>													
08:00:00 AM	08:15:00 AM												
08:15:00 AM	08:30:00 AM												
08:30:00 AM	08:45:00 AM												
08:45:00 AM	09:00:00 AM												
<b>2022</b>			8	1	0	7					2		1
<b>2027</b>			9	1	0	8					2		1
<b>DISTRIBUTION</b>													
08:00:00 AM	09:00:00 AM												
<b>SITE GENERATED TRAFFIC</b>					1								3
<b>TOTAL TRAFFIC 2027</b>			9	1	1	8					2		4
08:00:00 AM	09:00:00 AM		9	1	1	8					2		4

PM PEAK		WALTER PURDY AVENUE						MOSHER STREET					
ENTER		SOUTHBOUND			NORTHBOUND			WESTBOUND			EASTBOUND		
EXIT		L	T	R	L	T	R	L	T	R	L	T	R
<b>2022</b>													
04:15:00 PM	04:30:00 PM												
04:30:00 PM	04:45:00 PM												
04:45:00 PM	05:00:00 PM												
05:00:00 PM	05:15:00 PM												
<b>2022</b>			8	2	0	4					1		1
<b>2027</b>			9	2	0	4					1		1
<b>DISTRIBUTION</b>													
04:15:00 PM	05:15:00 PM												
<b>SITE GENERATED TRAFFIC</b>					2								2
<b>TOTAL TRAFFIC 2027</b>			9	2	2	4					1		3
04:15:00 PM	05:15:00 PM		9	2	2	4					1		3

# LOT 2021-2R AMHERST TRAFFIC IMPACT STUDY

## TOTAL TRAFFIC ANALYSIS

PEAK HOUR TRAFFIC COUNTS COMPLETED ON MONDAY JUNE 20, 2022

AM PEAK		WALTER PURDY AVENUE						WILLOW STREET					
ENTER		NORTHBOUND						WESTBOUND			EASTBOUND		
EXIT		L	T	R	L	T	R	L	T	R	L	T	R
<b>2022</b>													
08:00:00 AM	08:15:00 AM	2		3				1	12			38	0
08:15:00 AM	08:30:00 AM	0		0				2	36			61	0
08:30:00 AM	08:45:00 AM	1		3				2	49			75	2
08:45:00 AM	09:00:00 AM	1		1				0	22			26	2
<b>2022</b>													
08:00:00 AM	09:00:00 AM	4		7				5	119			200	4
<b>2027</b>													
08:00:00 AM	09:00:00 AM	4		8				6	131			221	4
<b>DISTRIBUTION</b>													
08:00:00 AM	09:00:00 AM												
<b>SITE GENERATED TRAFFIC</b>													
08:00:00 AM	09:00:00 AM								5			21	
<b>TOTAL TRAFFIC 2027</b>													
08:00:00 AM	09:00:00 AM	4		8				6	136			242	4

PM PEAK		WALTER PURDY AVENUE						WILLOW STREET					
ENTER		NORTHBOUND						WESTBOUND			EASTBOUND		
EXIT		L	T	R	L	T	R	L	T	R	L	T	R
<b>2022</b>													
04:15:00 PM	04:30:00 PM	0		0				3	35			18	0
04:30:00 PM	04:45:00 PM	1		0				3	20			12	1
04:45:00 PM	05:00:00 PM	0		1				1	31			17	0
05:00:00 PM	05:15:00 PM	2		1				0	29			27	2
<b>2022</b>													
04:15:00 PM	05:15:00 PM	3		2				7	115			74	3
<b>2027</b>													
04:15:00 PM	05:15:00 PM	3		2				8	127			82	3
<b>DISTRIBUTION</b>													
04:15:00 PM	05:15:00 PM												
<b>SITE GENERATED TRAFFIC</b>													
04:15:00 PM	05:15:00 PM								19			8	
<b>TOTAL TRAFFIC 2027</b>													
04:15:00 PM	05:15:00 PM	3		2				8	146			90	3

# LOT 2021-2R AMHERST TRAFFIC IMPACT STUDY

## TOTAL TRAFFIC ANALYSIS

PEAK HOUR TRAFFIC COUNTS COMPLETED ON MONDAY JUNE 20, 2022

AM PEAK		GALLAGHER STREET						WILLOW STREET					
ENTER	16	NORTHBOUND						WESTBOUND			EASTBOUND		
EXIT	44	L	T	R	L	T	R	L	T	R	L	T	R

2022														
08:00:00 AM	08:15:00 AM													
08:15:00 AM	08:30:00 AM													
08:30:00 AM	08:45:00 AM													
08:45:00 AM	09:00:00 AM													
2022														
08:00:00 AM	09:00:00 AM								123			204		
2027														
08:00:00 AM	09:00:00 AM								136			225		
DISTRIBUTION														
08:00:00 AM	09:00:00 AM								38%			62%		
SITE GENERATED TRAFFIC														
08:00:00 AM	09:00:00 AM	13			21				5					8
TOTAL TRAFFIC 2027														
08:00:00 AM	09:00:00 AM	13			21				5	136			225	8

PM PEAK		GALLAGHER STREET						WILLOW STREET					
ENTER	39	NORTHBOUND						WESTBOUND			EASTBOUND		
EXIT	25	L	T	R	L	T	R	L	T	R	L	T	R

2022														
04:15:00 PM	04:30:00 PM													
04:30:00 PM	04:45:00 PM													
04:45:00 PM	05:00:00 PM													
05:00:00 PM	05:15:00 PM													
2022														
04:15:00 PM	05:15:00 PM								118			77		
2027														
04:15:00 PM	05:15:00 PM								130			85		
DISTRIBUTION														
04:15:00 PM	05:15:00 PM								61%			39%		
SITE GENERATED TRAFFIC														
04:15:00 PM	05:15:00 PM	12			8				19					13
TOTAL TRAFFIC 2027														
04:15:00 PM	05:15:00 PM	12			8				19	130			85	13

2022 AM PEAK EXISTING TRAFFIC  
5: Donald Street & Walter Purdy Avenue

07-09-2022

Intersection						
Int Delay, s/veh	1.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	5	39	52	2	4	6
Future Vol, veh/h	5	39	52	2	4	6
Conflicting Peds, #/hr	10	0	0	10	10	10
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	64	64	64	64	64	64
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	8	61	81	3	6	9

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	94	0	-	0	180
Stage 1	-	-	-	-	93
Stage 2	-	-	-	-	87
Critical Hdwy	4.12	-	-	-	6.42
Critical Hdwy Stg 1	-	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	-	5.42
Follow-up Hdwy	2.218	-	-	-	3.518
Pot Cap-1 Maneuver	1500	-	-	-	810
Stage 1	-	-	-	-	931
Stage 2	-	-	-	-	936
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1487	-	-	-	792
Mov Cap-2 Maneuver	-	-	-	-	792
Stage 1	-	-	-	-	918
Stage 2	-	-	-	-	929

Approach	EB	WB	SB
HCM Control Delay, s	0.8	0	9.2
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1487	-	-	-	873
HCM Lane V/C Ratio	0.005	-	-	-	0.018
HCM Control Delay (s)	7.4	0	-	-	9.2
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0.1

2027 AM PEAK BACKGROUND TRAFFIC  
5: Donald Street & Walter Purdy Avenue

07-15-2022

Intersection						
Int Delay, s/veh	1.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	6	43	57	2	4	7
Future Vol, veh/h	6	43	57	2	4	7
Conflicting Peds, #/hr	10	0	0	10	10	10
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	64	64	64	64	64	64
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	9	67	89	3	6	11

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	102	0	-	0	196 111
Stage 1	-	-	-	-	101 -
Stage 2	-	-	-	-	95 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1490	-	-	-	793 942
Stage 1	-	-	-	-	923 -
Stage 2	-	-	-	-	929 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1477	-	-	-	776 926
Mov Cap-2 Maneuver	-	-	-	-	776 -
Stage 1	-	-	-	-	910 -
Stage 2	-	-	-	-	922 -

Approach	EB	WB	SB
HCM Control Delay, s	0.9	0	9.2
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1477	-	-	-	865
HCM Lane V/C Ratio	0.006	-	-	-	0.02
HCM Control Delay (s)	7.5	0	-	-	9.2
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0.1

2027 AM PEAK TOTAL TRAFFIC  
5: Donald Street & Walter Purdy Avenue

07-09-2022

Intersection						
Int Delay, s/veh	1.9					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	7	43	57	4	9	12
Future Vol, veh/h	7	43	57	4	9	12
Conflicting Peds, #/hr	10	0	0	10	10	10
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	64	64	64	64	64	64
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	11	67	89	6	14	19

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	105	0	-	0	201
Stage 1	-	-	-	-	102
Stage 2	-	-	-	-	99
Critical Hdwy	4.12	-	-	-	6.42
Critical Hdwy Stg 1	-	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	-	5.42
Follow-up Hdwy	2.218	-	-	-	3.518
Pot Cap-1 Maneuver	1486	-	-	-	788
Stage 1	-	-	-	-	922
Stage 2	-	-	-	-	925
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1473	-	-	-	769
Mov Cap-2 Maneuver	-	-	-	-	769
Stage 1	-	-	-	-	907
Stage 2	-	-	-	-	918

Approach	EB	WB	SB
HCM Control Delay, s	1	0	9.4
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1473	-	-	-	851
HCM Lane V/C Ratio	0.007	-	-	-	0.039
HCM Control Delay (s)	7.5	0	-	-	9.4
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0.1

2022 PM PEAK EXISTING TRAFFIC  
2: Walter Purdy Avenue & Brown Street

07-09-2022

Intersection						
Int Delay, s/veh	1.2					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	1	0	1	3	6	2
Future Vol, veh/h	1	0	1	3	6	2
Conflicting Peds, #/hr	10	10	10	0	0	10
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	84	84	84	84	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	0	1	4	7	2

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	34	28	19	0	0
Stage 1	18	-	-	-	-
Stage 2	16	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	979	1047	1597	-	-
Stage 1	1005	-	-	-	-
Stage 2	1007	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	962	1029	1583	-	-
Mov Cap-2 Maneuver	962	-	-	-	-
Stage 1	996	-	-	-	-
Stage 2	999	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	8.7	1.8	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1583	-	962	-	-
HCM Lane V/C Ratio	0.001	-	0.001	-	-
HCM Control Delay (s)	7.3	0	8.7	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

2027 PM PEAK BACKGROUND TRAFFIC  
5: Donald Street & Walter Purdy Avenue

07-10-2022

Intersection						
Int Delay, s/veh	1.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	4	22	22	0	2	2
Future Vol, veh/h	4	22	22	0	2	2
Conflicting Peds, #/hr	10	0	0	10	10	10
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	84	84	84	84	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	5	26	26	0	2	2

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	36	0	-	0	82 46
Stage 1	-	-	-	-	36 -
Stage 2	-	-	-	-	46 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1575	-	-	-	920 1023
Stage 1	-	-	-	-	986 -
Stage 2	-	-	-	-	976 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1562	-	-	-	903 1006
Mov Cap-2 Maneuver	-	-	-	-	903 -
Stage 1	-	-	-	-	975 -
Stage 2	-	-	-	-	968 -

Approach	EB	WB	SB
HCM Control Delay, s	1.1	0	8.8
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1562	-	-	-	952
HCM Lane V/C Ratio	0.003	-	-	-	0.005
HCM Control Delay (s)	7.3	0	-	-	8.8
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0

2027 PM PEAK TOTAL TRAFFIC  
5: Donald Street & Walter Purdy Avenue

07-09-2022

Intersection						
Int Delay, s/veh	2.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↶	↷		↶	
Traffic Vol, veh/h	8	22	22	3	5	4
Future Vol, veh/h	8	22	22	3	5	4
Conflicting Peds, #/hr	10	0	0	10	10	10
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	84	84	84	84	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	10	26	26	4	6	5

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	40	0	-	0	94
Stage 1	-	-	-	-	38
Stage 2	-	-	-	-	56
Critical Hdwy	4.12	-	-	-	6.42
Critical Hdwy Stg 1	-	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	-	5.42
Follow-up Hdwy	2.218	-	-	-	3.518
Pot Cap-1 Maneuver	1570	-	-	-	906
Stage 1	-	-	-	-	984
Stage 2	-	-	-	-	967
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1557	-	-	-	885
Mov Cap-2 Maneuver	-	-	-	-	885
Stage 1	-	-	-	-	969
Stage 2	-	-	-	-	959

Approach	EB	WB	SB
HCM Control Delay, s	2	0	8.9
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1557	-	-	-	934
HCM Lane V/C Ratio	0.006	-	-	-	0.011
HCM Control Delay (s)	7.3	0	-	-	8.9
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0

2022 AM PEAK EXISTING TRAFFIC  
2: Walter Purdy Avenue & Brown Street

07-09-2022

Intersection						
Int Delay, s/veh	1.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	1	1	1	6	8	0
Future Vol, veh/h	1	1	1	6	8	0
Conflicting Peds, #/hr	10	10	10	0	0	10
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	64	64	64	64	64	64
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	2	2	2	9	13	0

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	46	33	23	0	-	0
Stage 1	23	-	-	-	-	-
Stage 2	23	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	964	1041	1592	-	-	-
Stage 1	1000	-	-	-	-	-
Stage 2	1000	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	948	1023	1579	-	-	-
Mov Cap-2 Maneuver	948	-	-	-	-	-
Stage 1	991	-	-	-	-	-
Stage 2	992	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	8.7	1	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1579	-	984	-	-
HCM Lane V/C Ratio	0.001	-	0.003	-	-
HCM Control Delay (s)	7.3	0	8.7	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

2027 AM PEAK BACKGROUND TRAFFIC  
2: Walter Purdy Avenue & Brown Street

07-10-2022

Intersection						
Int Delay, s/veh	1.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		
Traffic Vol, veh/h	1	1	1	7	9	0
Future Vol, veh/h	1	1	1	7	9	0
Conflicting Peds, #/hr	10	10	10	0	0	10
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	64	64	64	64	64	64
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	2	2	2	11	14	0

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	49	34	24	0	0
Stage 1	24	-	-	-	-
Stage 2	25	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	960	1039	1591	-	-
Stage 1	999	-	-	-	-
Stage 2	998	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	944	1021	1578	-	-
Mov Cap-2 Maneuver	944	-	-	-	-
Stage 1	990	-	-	-	-
Stage 2	990	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	8.7	0.9	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1578	-	981	-	-
HCM Lane V/C Ratio	0.001	-	0.003	-	-
HCM Control Delay (s)	7.3	0	8.7	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

2027 AM PEAK TOTAL TRAFFIC  
2: Walter Purdy Avenue & Brown Street

07-09-2022

Intersection						
Int Delay, s/veh	3.7					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	1	8	4	7	9	0
Future Vol, veh/h	1	8	4	7	9	0
Conflicting Peds, #/hr	10	10	10	0	0	10
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	64	64	64	64	64	64
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	2	13	6	11	14	0

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	57	34	24	0	0
Stage 1	24	-	-	-	-
Stage 2	33	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	950	1039	1591	-	-
Stage 1	999	-	-	-	-
Stage 2	989	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	931	1021	1578	-	-
Mov Cap-2 Maneuver	931	-	-	-	-
Stage 1	987	-	-	-	-
Stage 2	981	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	8.6	2.7	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1578	-	1010	-	-
HCM Lane V/C Ratio	0.004	-	0.014	-	-
HCM Control Delay (s)	7.3	0	8.6	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

2022 PM PEAK EXISTING TRAFFIC  
2: Walter Purdy Avenue & Brown Street

07-09-2022

Intersection						
Int Delay, s/veh	1.2					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	1	0	1	3	6	2
Future Vol, veh/h	1	0	1	3	6	2
Conflicting Peds, #/hr	10	10	10	0	0	10
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	84	84	84	84	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	0	1	4	7	2

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	34	28	19	0	0
Stage 1	18	-	-	-	-
Stage 2	16	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	979	1047	1597	-	-
Stage 1	1005	-	-	-	-
Stage 2	1007	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	962	1029	1583	-	-
Mov Cap-2 Maneuver	962	-	-	-	-
Stage 1	996	-	-	-	-
Stage 2	999	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	8.7	1.8	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1583	-	962	-	-
HCM Lane V/C Ratio	0.001	-	0.001	-	-
HCM Control Delay (s)	7.3	0	8.7	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

2027 PM PEAK BACKGROUND TRAFFIC  
2: Walter Purdy Avenue & Brown Street

07-10-2022

Intersection						
Int Delay, s/veh	1.1					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	1	0	1	3	7	2
Future Vol, veh/h	1	0	1	3	7	2
Conflicting Peds, #/hr	10	10	10	0	0	10
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	84	84	84	84	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	0	1	4	8	2

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	35	29	20	0	0
Stage 1	19	-	-	-	-
Stage 2	16	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	978	1046	1596	-	-
Stage 1	1004	-	-	-	-
Stage 2	1007	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	961	1028	1582	-	-
Mov Cap-2 Maneuver	961	-	-	-	-
Stage 1	995	-	-	-	-
Stage 2	999	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	8.8	1.8	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1582	-	961	-	-
HCM Lane V/C Ratio	0.001	-	0.001	-	-
HCM Control Delay (s)	7.3	0	8.8	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

2027 PM PEAK TOTAL TRAFFIC  
2: Walter Purdy Avenue & Brown Street

07-09-2022

Intersection						
Int Delay, s/veh	3.6					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	1	3	6	3	7	2
Future Vol, veh/h	1	3	6	3	7	2
Conflicting Peds, #/hr	10	10	10	0	0	10
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	84	84	84	84	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	4	7	4	8	2

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	47	29	20	0	0
Stage 1	19	-	-	-	-
Stage 2	28	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	963	1046	1596	-	-
Stage 1	1004	-	-	-	-
Stage 2	995	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	944	1028	1582	-	-
Mov Cap-2 Maneuver	944	-	-	-	-
Stage 1	992	-	-	-	-
Stage 2	987	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	8.6	4.9	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1582	-	1006	-	-
HCM Lane V/C Ratio	0.005	-	0.005	-	-
HCM Control Delay (s)	7.3	0	8.6	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

2022 AM PEAK EXISTING TRAFFIC  
1: Walter Purdy Avenue & Mosher Street

07-09-2022

Intersection						
Int Delay, s/veh	1.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	2	1	0	7	8	1
Future Vol, veh/h	2	1	0	7	8	1
Conflicting Peds, #/hr	10	10	10	0	0	10
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	64	64	64	64	64	64
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	3	2	0	11	13	2

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	45	34	25	0	0
Stage 1	24	-	-	-	-
Stage 2	21	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	965	1039	1589	-	-
Stage 1	999	-	-	-	-
Stage 2	1002	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	950	1021	1576	-	-
Mov Cap-2 Maneuver	950	-	-	-	-
Stage 1	991	-	-	-	-
Stage 2	994	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	8.7	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1576	-	973	-	-
HCM Lane V/C Ratio	-	-	0.005	-	-
HCM Control Delay (s)	0	-	8.7	-	-
HCM Lane LOS	A	-	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

2027 AM PEAK BACKGROUND TRAFFIC  
1: Walter Purdy Avenue & Mosher Street

07-10-2022

Intersection						
Int Delay, s/veh	1.2					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	2	1	0	8	9	1
Future Vol, veh/h	2	1	0	8	9	1
Conflicting Peds, #/hr	10	10	10	0	0	10
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	64	64	64	64	64	64
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	3	2	0	13	14	2

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	48	35	26	0	0
Stage 1	25	-	-	-	-
Stage 2	23	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	962	1038	1588	-	-
Stage 1	998	-	-	-	-
Stage 2	1000	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	947	1020	1575	-	-
Mov Cap-2 Maneuver	947	-	-	-	-
Stage 1	990	-	-	-	-
Stage 2	992	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	8.7	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1575	-	970	-	-
HCM Lane V/C Ratio	-	-	0.005	-	-
HCM Control Delay (s)	0	-	8.7	-	-
HCM Lane LOS	A	-	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

2027 AM PEAK TOTAL TRAFFIC  
1: Walter Purdy Avenue & Mosher Street

07-09-2022

Intersection						
Int Delay, s/veh	2.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	2	4	1	8	9	1
Future Vol, veh/h	2	4	1	8	9	1
Conflicting Peds, #/hr	10	10	10	0	0	10
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	64	64	64	64	64	64
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	3	6	2	13	14	2

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	52	35	26	0	0
Stage 1	25	-	-	-	-
Stage 2	27	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	957	1038	1588	-	-
Stage 1	998	-	-	-	-
Stage 2	996	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	941	1020	1575	-	-
Mov Cap-2 Maneuver	941	-	-	-	-
Stage 1	989	-	-	-	-
Stage 2	988	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	8.7	0.8	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1575	-	992	-	-
HCM Lane V/C Ratio	0.001	-	0.009	-	-
HCM Control Delay (s)	7.3	0	8.7	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

2022 PM PEAK EXISTING TRAFFIC  
1: Walter Purdy Avenue & Mosher Street

07-09-2022

Intersection						
Int Delay, s/veh	1.1					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	1	1	0	4	8	2
Future Vol, veh/h	1	1	0	4	8	2
Conflicting Peds, #/hr	10	10	10	0	0	10
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	84	84	84	84	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	1	0	5	10	2

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	36	31	22	0	0
Stage 1	21	-	-	-	-
Stage 2	15	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	977	1043	1593	-	-
Stage 1	1002	-	-	-	-
Stage 2	1008	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	961	1025	1580	-	-
Mov Cap-2 Maneuver	961	-	-	-	-
Stage 1	994	-	-	-	-
Stage 2	1000	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	8.6	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1580	-	992	-	-
HCM Lane V/C Ratio	-	-	0.002	-	-
HCM Control Delay (s)	0	-	8.6	-	-
HCM Lane LOS	A	-	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

2027 PM PEAK BACKGROUND TRAFFIC  
1: Walter Purdy Avenue & Mosher Street

07-10-2022

Intersection						
Int Delay, s/veh	1					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	1	1	0	4	9	2
Future Vol, veh/h	1	1	0	4	9	2
Conflicting Peds, #/hr	10	10	10	0	0	10
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	84	84	84	84	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	1	0	5	11	2

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	37	32	23	0	-	0
Stage 1	22	-	-	-	-	-
Stage 2	15	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	975	1042	1592	-	-	-
Stage 1	1001	-	-	-	-	-
Stage 2	1008	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	959	1024	1579	-	-	-
Mov Cap-2 Maneuver	959	-	-	-	-	-
Stage 1	993	-	-	-	-	-
Stage 2	1000	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	8.6	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1579	-	990	-	-
HCM Lane V/C Ratio	-	-	0.002	-	-
HCM Control Delay (s)	0	-	8.6	-	-
HCM Lane LOS	A	-	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

2027 PM PEAK TOTAL TRAFFIC  
1: Walter Purdy Avenue & Mosher Street

07-09-2022

Intersection						
Int Delay, s/veh	2.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	1	3	2	4	9	2
Future Vol, veh/h	1	3	2	4	9	2
Conflicting Peds, #/hr	10	10	10	0	0	10
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	84	84	84	84	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	4	2	5	11	2

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	41	32	23	0	-	0
Stage 1	22	-	-	-	-	-
Stage 2	19	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	970	1042	1592	-	-	-
Stage 1	1001	-	-	-	-	-
Stage 2	1004	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	954	1024	1579	-	-	-
Mov Cap-2 Maneuver	954	-	-	-	-	-
Stage 1	992	-	-	-	-	-
Stage 2	996	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	8.6	2.4	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1579	-	1006	-	-
HCM Lane V/C Ratio	0.002	-	0.005	-	-
HCM Control Delay (s)	7.3	0	8.6	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

2022 AM PEAK EXISTING TRAFFIC  
8: Walter Purdy Avenue & Willow Street

07-09-2022

Intersection						
Int Delay, s/veh	0.5					
Movement	SET	SER	NWL	NWT	NEL	NER
Lane Configurations						
Traffic Vol, veh/h	200	4	5	119	4	7
Future Vol, veh/h	200	4	5	119	4	7
Conflicting Peds, #/hr	0	10	10	0	10	10
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	64	64	64	64	64	64
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	313	6	8	186	6	11

Major/Minor	Major1	Major2	Minor1		
Conflicting Flow All	0	0	329	0	538 336
Stage 1	-	-	-	-	326 -
Stage 2	-	-	-	-	212 -
Critical Hdwy	-	-	4.12	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	-	-	2.218	-	3.518 3.318
Pot Cap-1 Maneuver	-	-	1231	-	504 706
Stage 1	-	-	-	-	731 -
Stage 2	-	-	-	-	823 -
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1221	-	492 694
Mov Cap-2 Maneuver	-	-	-	-	492 -
Stage 1	-	-	-	-	725 -
Stage 2	-	-	-	-	811 -

Approach	SE	NW	NE
HCM Control Delay, s	0	0.3	11.1
HCM LOS			B

Minor Lane/Major Mvmt	NELn1	NWL	NWT	SET	SER
Capacity (veh/h)	604	1221	-	-	-
HCM Lane V/C Ratio	0.028	0.006	-	-	-
HCM Control Delay (s)	11.1	8	0	-	-
HCM Lane LOS	B	A	A	-	-
HCM 95th %tile Q(veh)	0.1	0	-	-	-

2027 AM PEAK BACKGROUND TRAFFIC  
8: Walter Purdy Avenue & Willow Street

07-10-2022

Intersection						
Int Delay, s/veh	0.5					
Movement	SET	SER	NWL	NWT	NEL	NER
Lane Configurations						
Traffic Vol, veh/h	221	4	6	131	4	8
Future Vol, veh/h	221	4	6	131	4	8
Conflicting Peds, #/hr	0	10	10	0	10	10
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	64	64	64	64	64	64
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	345	6	9	205	6	13

Major/Minor	Major1	Major2	Minor1		
Conflicting Flow All	0	0	361	0	591 368
Stage 1	-	-	-	-	358 -
Stage 2	-	-	-	-	233 -
Critical Hdwy	-	-	4.12	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	-	-	2.218	-	3.518 3.318
Pot Cap-1 Maneuver	-	-	1198	-	470 677
Stage 1	-	-	-	-	707 -
Stage 2	-	-	-	-	806 -
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1188	-	458 666
Mov Cap-2 Maneuver	-	-	-	-	458 -
Stage 1	-	-	-	-	701 -
Stage 2	-	-	-	-	792 -

Approach	SE	NW	NE
HCM Control Delay, s	0	0.4	11.4
HCM LOS			B

Minor Lane/Major Mvmt	NELn1	NWL	NWT	SET	SER
Capacity (veh/h)	578	1188	-	-	-
HCM Lane V/C Ratio	0.032	0.008	-	-	-
HCM Control Delay (s)	11.4	8.1	0	-	-
HCM Lane LOS	B	A	A	-	-
HCM 95th %tile Q(veh)	0.1	0	-	-	-

2027 AM PEAK TOTAL TRAFFIC  
8: Walter Purdy Avenue & Willow Street

07-09-2022

Intersection						
Int Delay, s/veh	0.5					
Movement	SET	SER	NWL	NWT	NEL	NER
Lane Configurations						
Traffic Vol, veh/h	242	4	6	136	4	8
Future Vol, veh/h	242	4	6	136	4	8
Conflicting Peds, #/hr	0	10	10	0	10	10
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	64	64	64	64	64	64
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	378	6	9	213	6	13

Major/Minor	Major1	Major2	Minor1		
Conflicting Flow All	0	0	394	0	632 401
Stage 1	-	-	-	-	391 -
Stage 2	-	-	-	-	241 -
Critical Hdwy	-	-	4.12	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	-	-	2.218	-	3.518 3.318
Pot Cap-1 Maneuver	-	-	1165	-	444 649
Stage 1	-	-	-	-	683 -
Stage 2	-	-	-	-	799 -
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1155	-	433 638
Mov Cap-2 Maneuver	-	-	-	-	433 -
Stage 1	-	-	-	-	678 -
Stage 2	-	-	-	-	785 -

Approach	SE	NW	NE
HCM Control Delay, s	0	0.3	11.8
HCM LOS			B

Minor Lane/Major Mvmt	NELn1	NWL	NWT	SET	SER
Capacity (veh/h)	551	1155	-	-	-
HCM Lane V/C Ratio	0.034	0.008	-	-	-
HCM Control Delay (s)	11.8	8.1	0	-	-
HCM Lane LOS	B	A	A	-	-
HCM 95th %tile Q(veh)	0.1	0	-	-	-

2022 PM PEAK EXISTING TRAFFIC  
8: Walter Purdy Avenue & Willow Street

07-09-2022

Intersection						
Int Delay, s/veh	0.5					
Movement	SET	SER	NWL	NWT	NEL	NER
Lane Configurations						
Traffic Vol, veh/h	74	3	7	115	3	2
Future Vol, veh/h	74	3	7	115	3	2
Conflicting Peds, #/hr	0	10	10	0	10	10
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	84	84	84	84	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	88	4	8	137	4	2

Major/Minor	Major1	Major2	Minor1		
Conflicting Flow All	0	0	102	0	263 110
Stage 1	-	-	-	-	100 -
Stage 2	-	-	-	-	163 -
Critical Hdwy	-	-	4.12	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	-	-	2.218	-	3.518 3.318
Pot Cap-1 Maneuver	-	-	1490	-	726 943
Stage 1	-	-	-	-	924 -
Stage 2	-	-	-	-	866 -
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1477	-	710 927
Mov Cap-2 Maneuver	-	-	-	-	710 -
Stage 1	-	-	-	-	917 -
Stage 2	-	-	-	-	854 -

Approach	SE	NW	NE
HCM Control Delay, s	0	0.4	9.6
HCM LOS			A

Minor Lane/Major Mvmt	NELn1	NWL	NWT	SET	SER
Capacity (veh/h)	783	1477	-	-	-
HCM Lane V/C Ratio	0.008	0.006	-	-	-
HCM Control Delay (s)	9.6	7.5	0	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	0	-	-	-

2027 PM PEAK BACKGROUND TRAFFIC  
8: Walter Purdy Avenue & Willow Street

07-10-2022

Intersection						
Int Delay, s/veh	0.5					
Movement	SET	SER	NWL	NWT	NEL	NER
Lane Configurations						
Traffic Vol, veh/h	82	3	8	127	3	2
Future Vol, veh/h	82	3	8	127	3	2
Conflicting Peds, #/hr	0	10	10	0	10	10
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	84	84	84	84	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	98	4	10	151	4	2

Major/Minor	Major1	Major2	Minor1	Minor2		
Conflicting Flow All	0	0	112	0	291	120
Stage 1	-	-	-	-	110	-
Stage 2	-	-	-	-	181	-
Critical Hdwy	-	-	4.12	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	2.218	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	1478	-	700	931
Stage 1	-	-	-	-	915	-
Stage 2	-	-	-	-	850	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1465	-	684	915
Mov Cap-2 Maneuver	-	-	-	-	684	-
Stage 1	-	-	-	-	908	-
Stage 2	-	-	-	-	837	-

Approach	SE	NW	NE
HCM Control Delay, s	0	0.4	9.8
HCM LOS			A

Minor Lane/Major Mvmt	NELn1	NWL	NWT	SET	SER
Capacity (veh/h)	761	1465	-	-	-
HCM Lane V/C Ratio	0.008	0.007	-	-	-
HCM Control Delay (s)	9.8	7.5	0	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	0	-	-	-

2027 PM PEAK TOTAL TRAFFIC  
8: Walter Purdy Avenue & Willow Street

07-09-2022

Intersection						
Int Delay, s/veh	0.4					
Movement	SET	SER	NWL	NWT	NEL	NER
Lane Configurations						
Traffic Vol, veh/h	90	3	8	146	3	2
Future Vol, veh/h	90	3	8	146	3	2
Conflicting Peds, #/hr	0	10	10	0	10	10
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	84	84	84	84	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	107	4	10	174	4	2

Major/Minor	Major1	Major2	Minor1		
Conflicting Flow All	0	0	121	0	323
Stage 1	-	-	-	-	119
Stage 2	-	-	-	-	204
Critical Hdwy	-	-	4.12	-	6.42
Critical Hdwy Stg 1	-	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	-	5.42
Follow-up Hdwy	-	-	2.218	-	3.518
Pot Cap-1 Maneuver	-	-	1467	-	671
Stage 1	-	-	-	-	906
Stage 2	-	-	-	-	830
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1455	-	655
Mov Cap-2 Maneuver	-	-	-	-	655
Stage 1	-	-	-	-	899
Stage 2	-	-	-	-	817

Approach	SE	NW	NE
HCM Control Delay, s	0	0.4	9.9
HCM LOS			A

Minor Lane/Major Mvmt	NELn1	NWL	NWT	SET	SER
Capacity (veh/h)	736	1455	-	-	-
HCM Lane V/C Ratio	0.008	0.007	-	-	-
HCM Control Delay (s)	9.9	7.5	0	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	0	-	-	-

2027 AM PEAK TOTAL TRAFFIC  
4: Gallagher Street & Willow Street

07-09-2022

Intersection						
Int Delay, s/veh	1.1					
Movement	SET	SER	NWL	NWT	NEL	NER
Lane Configurations						
Traffic Vol, veh/h	225	8	5	136	13	21
Future Vol, veh/h	225	8	5	136	13	21
Conflicting Peds, #/hr	0	10	10	0	10	10
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	64	64	64	64	64	64
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	352	13	8	213	20	33

Major/Minor	Major1	Major2	Minor1		
Conflicting Flow All	0	0	375	0	608 379
Stage 1	-	-	-	-	369 -
Stage 2	-	-	-	-	239 -
Critical Hdwy	-	-	4.12	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	-	-	2.218	-	3.518 3.318
Pot Cap-1 Maneuver	-	-	1183	-	459 668
Stage 1	-	-	-	-	699 -
Stage 2	-	-	-	-	801 -
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1173	-	448 657
Mov Cap-2 Maneuver	-	-	-	-	448 -
Stage 1	-	-	-	-	693 -
Stage 2	-	-	-	-	788 -

Approach	SE	NW	NE
HCM Control Delay, s	0	0.3	12.1
HCM LOS			B

Minor Lane/Major Mvmt	NELn1	NWL	NWT	SET	SER
Capacity (veh/h)	558	1173	-	-	-
HCM Lane V/C Ratio	0.095	0.007	-	-	-
HCM Control Delay (s)	12.1	8.1	0	-	-
HCM Lane LOS	B	A	A	-	-
HCM 95th %tile Q(veh)	0.3	0	-	-	-

2027 PM PEAK TOTAL TRAFFIC  
4: Gallagher Street & Willow Street

07-09-2022

Intersection						
Int Delay, s/veh	1.3					
Movement	SET	SER	NWL	NWT	NEL	NER
Lane Configurations						
Traffic Vol, veh/h	85	13	19	130	12	8
Future Vol, veh/h	85	13	19	130	12	8
Conflicting Peds, #/hr	0	10	10	0	10	10
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	84	84	84	84	84	84
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	101	15	23	155	14	10

Major/Minor	Major1	Major2	Minor1		
Conflicting Flow All	0	0	126	0	330
Stage 1	-	-	-	-	119
Stage 2	-	-	-	-	211
Critical Hdwy	-	-	4.12	-	6.42
Critical Hdwy Stg 1	-	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	-	5.42
Follow-up Hdwy	-	-	2.218	-	3.518
Pot Cap-1 Maneuver	-	-	1460	-	665
Stage 1	-	-	-	-	906
Stage 2	-	-	-	-	824
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1448	-	643
Mov Cap-2 Maneuver	-	-	-	-	643
Stage 1	-	-	-	-	899
Stage 2	-	-	-	-	803

Approach	SE	NW	NE
HCM Control Delay, s	0	1	10.1
HCM LOS			B

Minor Lane/Major Mvmt	NELn1	NWL	NWT	SET	SER
Capacity (veh/h)	727	1448	-	-	-
HCM Lane V/C Ratio	0.033	0.016	-	-	-
HCM Control Delay (s)	10.1	7.5	0	-	-
HCM Lane LOS	B	A	A	-	-
HCM 95th %tile Q(veh)	0.1	0	-	-	-

# SYNOPSIS

## Appointment of Development Officer

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The Municipal Government Act requires that in order to sign a development permit that person must be designated as a Development Officer by Council. Abiola Falaye has recently been hired as a Land Use Planner. He comes to the town with a Bachelor of Technology in Urban and Regional Planning, Master of Science in Urban Planning, a Master of Science (Double Degree) in Environmental Management, and a Graduate Certificate in Geospatial Data Analytics. Abiola also brings international work experience in both research and practice of urban and regional planning. Part of his work with the Town will be reviewing development applications and signing development permits as required.

**MOTION:**

**That Council appoint Abiola Falaye as a Development Officer for the Town of Amherst.**



**AMHERST TOWN COUNCIL**

**RFD# 2022072**

**Date: November 28, 2022**

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**TO:** Mayor Kogon and Members of Council

**SUBMITTED BY:** Andrew Fisher, Director of Planning & Strategic Initiatives

**DATE:** November 28, 2022

**SUBJECT:** **Development Officer appointment – Abiola Falaye**

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**ORIGIN:** Efforts to improve operational capacity, efficiency and customer service levels.

**LEGISLATIVE AUTHORITY:** MGA section 243 (1) *A council shall appoint a development officer to administer its land-use by-law and subdivision by-law.*

**RECOMMENDATION:** That Council appoint Abiola Falaye as a Development Officer for the Town of Amherst.

**BACKGROUND:** The Municipal Government Act requires that in order to sign a development permit that person must be designated as a Development Officer by Council. Abiola has recently been hired as a Land Use Planner. He comes to the town with a Bachelor of Technology in Urban and Regional Planning, Master of Science in Urban Planning, a Master of Science (Double Degree) in Environmental Management, and a Graduate Certificate in Geospatial Data Analytics (GIS). Abiola also brings international work experience in both research and practice of urban and regional planning.

**DISCUSSION:** With his appointment as Development Officer, Abiola will continue to be brought up to speed with development permitting processes and will gradually get into a position to be able to process applications with increasing complexity.

**FINANCIAL IMPLICATIONS:** None specific to this issue.

**SOCIAL JUSTICE IMPLICATIONS:** None specific to this issue.

**ENVIRONMENTAL IMPLICATIONS:** None specific this issue.

**COMMUNITY ENGAGEMENT:** None specific to this issue.

**ALTERNATIVES:** Do not make the appointment.

**ATTACHMENTS:** None.

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Report prepared by: A.Fisher

Report and Financial approved by:



# SYNOPSIS

## Stadium Canteen Agreement

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The stadium canteen has not been in service since it was required to close due to the Covid 19 pandemic. Over the past 12 months staff issued an RFP for operation of the canteen and no submissions were received. As there was no response to the RFP staff contacted local community groups and small business owners to gage their interest in operating the canteen. While some groups initially showed interest in possibly operating the canteen staff only received one proposal. That proposal was from local business owner Dwayne Ripley.

Mr. Ripley is the owner and operator of Curly's Sports & Supplements and The Balcony Candy Shoppe. Staff are confident that Mr. Ripley has the experience and ability to successfully operate the stadium canteen for the remainder of the 2022/23 season. The agreement will see Mr. Ripley operate the canteen services from December 1, 2022 to May 1, 2023.

### **MOTION:**

**That Council approve of the stadium canteen concession agreement between Dwayne Ripley and the Town of Amherst and authorize the Mayor and CAO to sign on behalf of the Town.**

---

**TO:** Mayor Kogon and Members of Council

**SUBMITTED BY:** Corey Crocker, Recreation Facilities Manager

**DATE:** November 28, 2022

**SUBJECT:** Canteen Concession Agreement

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**ORIGIN:** Expiration of the current canteen concession agreement at the Amherst Stadium.

**LEGISLATIVE AUTHORITY:** Municipal Government Act (MGA), Section 51 concerning the sale or lease of municipal property.

**RECOMMENDATION:** That Council approve of the stadium canteen concession agreement between Dwayne Ripley and the Town of Amherst and authorize the Mayor and CAO to sign on behalf of the Town.

**BACKGROUND:** The stadium canteen has not been in service since March 14, 2019. The reason for the closure at that time was due to the Covid 19 pandemic. An RFP for operation of the canteen was issued on two separate occasions and no submissions were received. As no submissions were received staff contacted local community groups and business owners to gauge their interest in operating the stadium canteen. While some groups initially showed interest in operating the canteen only one proposal was received, local business owner Dwayne Ripley.

**DISCUSSION:** Mr. Ripley is the owner and operator of Curly's Sports & Supplements and The Balcony Candy Shoppe. Staff are confident that Mr. Ripley has the experience and ability to successfully operate the stadium canteen for the remainder of the 2022/23 season. The proposed agreement would commence on December 1, 2022 and end on May 1, 2023.

**FINANCIAL IMPLICATIONS:** Mr. Ripley will pay a rental rate of \$500.00 plus HST per month for a period of 5 months, totalling \$2,500.00 plus HST.

**SOCIAL JUSTICE IMPLICATIONS:** Providing a canteen service at the Amherst Stadium will provide users and spectators a more positive experience while attending events at the facility.

**ENVIRONMENTAL IMPLICATIONS:** There are no environmental implications to opening the canteen at the Amherst Stadium.

**COMMUNITY ENGAGEMENT:** If the agreement is approved, staff will prepare a media release and promote the canteen re-opening on social media.

**ALTERNATIVES:**

1. Do not approve the proposed agreement and keep the canteen closed at this time.
2. Refer the proposed agreement back to staff for further negotiations with Mr. Ripley.





**AMHERST TOWN COUNCIL**

**RFD# 2022073**

**Date: November 28, 2022**

**ATTACHMENTS:** Proposed Canteen Concession Agreement

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Report prepared by: Corey Crocker, Recreation Facilities Manager  
Report and Financial approved by:



# Canteen Concession Agreement

**THIS AGREEMENT** made this \_\_\_\_\_ day of \_\_\_\_\_ 2022

**BETWEEN:**

**THE TOWN OF AMHERST** (the Town)

and

**DWAYNE RIPLEY** (the Operator)

**WHEREAS;** the Town and its assigns grant to the Operator the right to operate the food and drink concession at the Amherst Stadium for a term commencing December 1<sup>st</sup> 2022 to May 1<sup>st</sup> 2023; and

**WHEREAS** the Operator agrees to pay a rental rate of \$500.00 per month plus HST;

**THEREFORE,** it is agreed that the Operator shall agree to the terms and conditions of operation as set out in APPENDIX A.

**EXECUTED** at Amherst this \_\_\_\_\_ day of \_\_\_\_\_ 2022

**TOWN OF AMHERST**

\_\_\_\_\_  
Witness

Per: \_\_\_\_\_  
Jason MacDonald, CAO

Per: \_\_\_\_\_  
David Kogon, Mayor

\_\_\_\_\_  
Witness

\_\_\_\_\_  
**DWAYNE RIPLEY**

## Appendix A

1. The Operator may operate the primary canteen on the first level of the Amherst Stadium as well as the corner canteen area on the second level of the Amherst Stadium.
2. Both canteen locations will be open for all Amherst Jr. A Rambler games.
3. The Primary canteen will also be open for all major tournaments and events hosted at the Amherst Stadium. This would include but is not limited to events hosted by Cumberland County Minor Hockey and the Amherst Skating Club.
4. Rent is due and payable on the first day of the month.
5. The Operator agrees to open the main canteen on Saturday and Sunday, as the operating schedule warrants, and work towards opening during weekday nights during peak hours. The Operator and Facility Manager will work together to develop a mutually agreeable schedule.
6. Both parties agree that the priority is to have the canteen operational upon signing of the agreement and that hot food service may not be offered initially.
7. The Operator agrees to obtain and hold any food sales permits and/or food handler's certifications maybe required.
8. The Operator shall be responsible for processing, preparing, storing, and serving all food and beverage items adhering to all federal and provincial regulations.
9. The Operator may not assign (sub-contract) any of its rights or obligations without the prior written approval of the Town of Amherst.
10. The Operator shall not use the area of operation, nor shall permit others to use the area of operation, for any other purpose than the purposes of operating the concession to provide food and beverage service to facility users.
11. The Operator shall keep the area of operation clean, clear of waste, paper, garbage, combustible materials, and obstructions, and shall not cause or permit any noises and odors which would constitute a nuisance to emanate from the area of operation.
12. The Operator shall sort solid waste in accordance with the Town's Solid Waste Bylaw.
13. The Operator agrees to obtain and hold an insurance policy of a minimum of \$2,000,000 for the operation.
14. If the Operator refuses or fails to comply with any of the terms and conditions of this agreement, the Town shall have the right, at its sole option, to terminate this agreement forthwith by notice in writing to the Operator.
15. The Operator may terminate this agreement at any time by providing to the Town, in writing, 30 days' notice of the Operator's intention to terminate the agreement.

# SYNOPSIS

## Gritty to Pretty Program 2022-2023

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The provincial Beautification and Streetscaping Program provides funding for commercial building exterior improvements, as well as funding to make public spaces more attractive and vibrant. The funding program for 2022-2023 is for a total of \$25,000, with \$10,000 being allocated for the Gritty to Pretty program, which funds up to 50% exterior improvements up to a maximum of \$5,000. The remaining \$15,000 will be used to augment existing budget allocations to purchase additional “street furniture”, which may include everything from benches, 3-stream waste receptacles, Christmas lights, and more.

Several commercial properties in Amherst have benefited from improvements to facades, signage, and exterior lighting through the program in recent years. These improvements enhance Amherst’s tourism development by making public spaces more functional and inviting.

The program guidelines encourage barrier-free improvements with smooth ground-level entrances, wide doorways, clearly visible signage and handrails to facilitate accessibility for people with physical limitations.

The Memorandum of Understanding between the Town and the Amherst and Area Chamber of Commerce sets the terms and conditions for the Chamber to administer the Gritty to Pretty program with support from the Town’s Business Development Officer.

### **MOTION:**

**That the Memorandum of Understanding between the Town and the Amherst & Area Chamber of Commerce to facilitate the Gritty to Pretty Program for 2022-2023 be approved and the CAO be authorized to sign the agreement on the behalf of Council; and further, that staff be directed to determine street furniture purchasing opportunities and request any needed changes to the capital budget in due course.**

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**TO:** Mayor Kogon and Members of Council

**SUBMITTED BY:** Andrew Fisher, Director of Planning & Strategic Initiatives

**DATE:** November 28, 2022

**SUBJECT:** **Gritty to Pretty Funding Program**

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**ORIGIN:** The subject funding program supports Strategic Priorities such as ‘**facilitating business development**’, ‘**community vibrancy**’, and ‘**attracting people to live in Amherst**’.

**LEGISLATIVE AUTHORITY:** Municipal Government Act (MGA), section 65A (2) *The municipality may expend money provided for in an operating budget or capital budget for a purpose other than that set out in the operating budget or capital budget for that fiscal year if the expenditure does not affect the total of the amounts estimated for the operating budget and the capital budget.*

**RECOMMENDATION:** That the Memorandum of Understanding between the Town and the Amherst & Area Chamber of Commerce to facilitate the Gritty to Pretty Program for 2022-2023 be approved; and further, that staff be directed to determine street furniture purchasing opportunities and request any needed changes to the capital budget in due course.

**BACKGROUND:** The Department of Municipal Affairs and Housing (DMAH), under the Beautification and Streetscaping Program (BSP), provides funding for the Gritty to Pretty 2021/22 and 2022/23 Funding Programs.

The 21/22 total funding is \$25,000. The original March 31, 2022 program deadline was extended to December 31, 2022 due to delays caused by the pandemic. The \$25,000 is allocated into two categories:

- 1) \$18,000 - Gritty to Pretty Program, administered by the Chamber of Commerce, to fund commercial business exterior facade improvements to a maximum of \$3,000 that must be matched by the applicant.
- 2) \$7,000 to fund 50% of street furniture acquisitions purchased by the Town.

Note: the above allocations can be adjusted based on participation to use the total \$25,000 funding award so long as there are matching funds.

Approximately \$9000 of the \$18,000 in funding has been utilized by business participants. \$7000 was included in the TOA 21/22 budget for the purchase of street furniture, with the intended purchase of self-watering planters, which is currently in process. To utilize the remaining \$9000 in funding not utilized by the Gritty to Pretty 21/22 program, there are internal department discussions ongoing to find opportunities to use these funds prior to the program closing date.

Concurrent with the 21/22 funding program, another \$25,000 was awarded in July 2022 under the 22/23 Program. The \$25,000 is funded with the same 2 categories but with different fund levels of \$10,000 for Gritty to Pretty 22/23, \$15,000 for TOA street furniture acquisition. The 22/23 BSP funding closes on March 31st, 2023.

The 22/23 Gritty to Pretty Program, again being administered by the Chamber of Commerce, subject to the attached MOU, has been adjusted to provide business improvement projects up to \$5000 funding with matching funds coming from the recipient. Town staff are discussing opportunities to utilize the \$15,000 street furniture allocation.

As noted above, staff in all departments are looking at ways to augment existing budget allocations to purchase additional street furniture, which can include everything from benches, 3-stream waste receptacles, Christmas lights, and more. An RFD to council may be required based on the intended opportunities identified by staff and any implications to the existing budget.

**DISCUSSION:** Several commercial properties in Amherst have benefited from improvements to facades, signage, and exterior lighting. These improvements would enhance Amherst's tourism development. In addition, the opportunity to use 50% funding for street furniture helps support community vibrancy by making public spaces more functional and inviting.

**FINANCIAL IMPLICATIONS:** \$7000 was included in the 21/22 budget for the purchase of street furniture.

**SOCIAL JUSTICE IMPLICATIONS:** The program guidelines encourage barrier-free improvements with smooth ground-level entrances, wide doorways, clearly visible signage and handrails to facilitate accessibility for people with physical limitations.

**ENVIRONMENTAL IMPLICATIONS:** None specific to this issue.

**COMMUNITY ENGAGEMENT:** Commercial property owners throughout Amherst were contacted through the process to determine an interest in participating.



**AMHERST TOWN COUNCIL**

**RFD# 2022076**

**Date: November 28, 2022**

**ALTERNATIVES:** Do not participate in the 2022-23 funding program.

**ATTACHMENTS:** TOA / Chamber of Commerce MOU

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Report prepared by:

Report and Financial approved by:



# Memorandum of Understanding

Between: Town of Amherst (“Town”)  
And: Amherst & Area Chamber of Commerce (“Chamber”)  
RE: Gritty to Pretty Program 2022-2023 (“Program”)

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## Definitions

1. “Business premises” means any building used for a primary purpose other than residential use.
2. “Façade” means the street-facing exterior side of a commercial building.
3. “Program Area” means all business premises located within the Town of Amherst.
4. “Program” means the Gritty to Pretty beautification and streetscaping program.
5. “Chamber” means Amherst & Area Chamber of Commerce.
6. “Town” means the Town of Amherst.

## Background

7. The Town of Amherst has successfully applied for financial assistance through the Nova Scotia government’s *Beautification and Streetscaping Program* to partner with a body corporate for the purpose of beautifying a business district with a specific focus on tourism attraction.
8. The Chamber is willing to partner with the Town of Amherst for the purpose of receiving the funding in order to promote and administer the program.

## Commitment by the Town

9. The Town will provide funding to the Chamber by way of a grant in an amount up to \$10,000. The Town will provide support to the Chamber through its Business Development Officer, who will advise and consult with the Chamber regarding the program.
10. The Town shall provide and approve a “Gritty to Pretty Program Design Guidelines & Information Package” with all associated supplemental information, forms, and reference documents necessary for the Chamber to properly administer the project.
11. The Town will have the right to audit the records of the Chamber to ensure that the project is managed according to the objectives of the Town.
12. The Town will provide guidelines to the Chamber for the parameters of the project, including the types of businesses that may apply for a grant, the amount that may be applied for, and the type of work that is eligible for a grant. In the event the Town makes any changes to the parameters of the project, the Town agrees to provide the Chamber with reasonable notice in order to implement the changes.

## Commitment by the Chamber

13. The Chamber will appoint a program coordinator to administer and manage the project in accordance with the “Gritty to Pretty Program Design Guidelines & Information Package”.
14. The Chamber will take all steps necessary to manage the application process in a timely and comprehensive manner.
15. The Chamber will establish a project review committee made up of three members of the Chamber’s Executive Committee of the Board of Directors. The project review committee will approve projects based on criteria in the “Gritty to Pretty Program Design Guidelines & Information Package”.

16. The Chamber will authorize payments pursuant to the project upon obtaining satisfactory evidence of the completion of the work in accordance with the application submitted.
17. The Chamber will report to the Town on a monthly basis with respect to its progress and administration of the project.
18. The Chamber shall maintain all records, documents, reference material, permits, paid invoices received with respect to the project, and will permit the Town to audit the project in its entirety.
19. The project will provide funding of up to \$5,000 per building for beautification in keeping with the “Gritty to Pretty Design Guidelines & Information Package”, based on matching funds being contributed by the applicant. In-kind contributions or work by contractors with whom the applicant has an interest, as defined by the Municipal Conflict of Interest Act, shall not be eligible for consideration.
20. The Chamber shall maintain sufficient insurance for the purposes of implementing the project, and provide the Town with a certificate of insurance naming the Town of Amherst as an additional named insured.

### Values

21. The Town and Chamber agree that the following values will apply to the project:
  - a. Citizen Engagement: The Town and Chamber will take all steps necessary to engage the citizens and business operators of the Town of Amherst to ensure that the project is successfully completed and improves the Town of Amherst as a whole.
  - b. Mutual Respect: The Town and Chamber hold the best interests of the Town of Amherst’s citizens and business operators at the forefront, and will mutually respect the other’s interest in the project.
  - c. Sustainability: The Town and Chamber agree that the project shall be carried out in a manner that represents a more sustainable future for the Town of Amherst.
  - d. Local Opportunities: The Town and Chamber agree to maximize the local benefits of the project undertaken to the extent possible.
  - e. Transparency: The Town and Chamber agree that the use of public funds and decisions concerning the investment thereof should be open and transparent to stakeholders.

### General Terms

22. This Memorandum of Understanding shall be adopted by the Town and by the Chamber and shall not be binding until so adopted.
23. The Chamber agrees that it shall adhere to the principals of accountability and transparency as required by the Town as set forward in the Municipal Government Act.
24. This Memorandum of Understanding shall expire on March 31, 2023, or until the program deadline if extended by the Nova Scotia government’s *Beautification and Streetscaping Program*.

In witness whereof, the parties have properly executed this Agreement this \_\_\_\_\_ day of \_\_\_\_\_, 2022.

SIGNED, SEALED & DELIVERED )  
In the presence of: )

\_\_\_\_\_  
Witness )

\_\_\_\_\_  
Witness )

**TOWN OF AMHERST**

\_\_\_\_\_  
Per: )

**AMHERST & AREA CHAMBER OF COMMERCE**

\_\_\_\_\_  
Per: )

# SYNOPSIS

## Adjustment to the 2022/23 Capital Budget

---

\$50,000 was included in the 2022/23 capital budget for a Stormwater Management Study. A Request for Proposals was issued and four proposals were received. The highest scoring proponent quoted a price of \$75,825.00 plus HST to complete the study.

In order to complete the study an additional \$30,000 in funding is required. The additional funding will come from the Operating Reserve.

The study, when complete, will provide the Town with recommendations to reduce the inflow of stormwater into the sanitary sewer system and provide recommendations to reduce the impacts of localized flooding during heavy rainfall events as was experienced in February 2022.

### **MOTION:**

**That Council approve amendments to the 2022/23 General Capital Budget to increase the approved budget amount in the Capital budget for the Stormwater Management Study from \$50,000 to \$80,000.**

---

**TO:** Mayor Kogon and Members of Council

**SUBMITTED BY:** Aaron Bourgeois, Director of Operations

**DATE:** November 28, 2022

**SUBJECT:** **Adjustment to the 2022/23 Capital Budget**

---

**ORIGIN:** Budget Management policy 03450-04

**LEGISLATIVE AUTHORITY:** MGA section 65.

**RECOMMENDATION:** That Council approve amendments to the 2022/23 General Capital Budget to increase the approved budget amount in the Capital budget for the Stormwater Management Study from \$50,000 to \$80,000.

**BACKGROUND:** \$50,000 was included in the 2022/23 capital budget (\$25,000 operating reserve and \$25,000 PCAP) for a Stormwater management Study. A request for Proposals was issued in October, 4 proposals were received. The highest scoring proponent has a quoted price of \$75,825.00 plus applicable taxes to complete the study.

**DISCUSSION:** As previously discussed with Council during the capital budget process, a stormwater management study was needed to address the flooding issues that occurred in February 2022. The study will focus on two areas in particular; South Albion Street near Industrial Park Drive and the Donald Avenue, Ralston Place and Dickey Street area.

**FINANCIAL IMPLICATIONS:** The \$30,000 increase in funding for the Stormwater Management Plan will come from the Operating Reserve, which is also the original funding source.

**SOCIAL JUSTICE IMPLICATIONS:** There are no social justice implications to the recommended course of action.

**ENVIRONMENTAL IMPLICATIONS:** The study will provide recommendations to reduce the inflow of stormwater into the sanitary sewer system and provide recommendations to reduce the impacts of localized flooding during heavy rainfall events.

**COMMUNITY ENGAGEMENT:** No community engagement is anticipated, other than the issuance of a media release.



**AMHERST TOWN COUNCIL**

**RFD# 2022071**

**Date: November 28, 2022**

**ALTERNATIVES:** Cancel the RFP and reissue it with a revised scope of work that can be accommodated within the existing approved budget amount.

**ATTACHMENTS:** None.

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Report prepared by: Aaron Bourgeois, Director of Operations  
Report and Financial approved by:



# SYNOPSIS

## Asphalt Recycler Purchase

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Town staff have been looking for different equipment to help them improve our efforts to repair potholes year-round as well as temporarily repair service cuts in streets during the winter.

Over the past year our Public Works staff have used an asphalt recycler borrowed from the Municipality of the County of Cumberland County and were very impressed with the efficiency of the equipment and the quality of the asphalt produced. Staff are recommending the purchase of a recycler in the upcoming capital budget at an estimated cost of \$195,000.

We have recently been made aware that SNT Solutions Inc. has a refurbished asphalt recycler currently available for sale. They are offering us the machine for \$80,000 plus HST, saving the Town approximately \$115,000 compared to the cost of a new recycler.

The purchase of this asphalt recycler will realize operational efficiencies as it can be loaded with asphalt and ready for use within 30 minutes. This will improve response time to pothole complaints, increase the number of potholes that can be patched during a workday and allow staff to repair service cuts during the winter.

### **MOTION:**

**That Council amend the current capital budget to include the purchase of a Bagela Asphalt Recycler from SNT Solutions Inc., at their quoted price of \$80,000 plus non-recoverable taxes, with funding of \$20,900 from the Water Utility Depreciation Fund and \$62,600 from the Operating Reserve – Unrestricted.**



**AMHERST TOWN COUNCIL**

**RFD# 2022082**

**Date: November 28, 2022**

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**TO:** Mayor Kogon and Members of Council

**SUBMITTED BY:** Aaron Bourgeois, Director of Operations

**DATE:** November 28, 2022

**SUBJECT:** Purchase of an Asphalt Recycler

---

**ORIGIN:** Review of operational efficiencies.

**LEGISLATIVE AUTHORITY:** MGA Section 65A (4)

**RECOMMENDATION:** That Council amend the current capital budget to include \$80,000, to be funded from the Operating Reserve for the purchase of a Bagela Asphalt Recycler from SNT Solutions Inc., at their quoted price of \$80,000 plus non-recoverable taxes.

**BACKGROUND:** SNT Solutions Inc. has a refurbished Bagela asphalt recycler available for sale. This machine is the same make and model as the one operations staff borrowed from the Municipality of the County of Cumberland in the Spring of 2022 to patch potholes. Staff and management were impressed with the efficiency and quality of the asphalt produced with the Bagela recycler.

The hot-box we currently have has not functioned as well as expected. The heating is not evenly distributed which renders much of the material cold and unusable. The unit does function well with hot mix directly from an asphalt plant or cold uncompressed virgin asphalt, neither of which are readily available in the winter.

**DISCUSSION:** The Bagela is a true asphalt recycler that has a drum like a concrete truck which tumbles the material to break it down and provides an even distribution of heat throughout the asphalt. The Bagela is also very efficient; the machine can be loaded, heated, and ready to use in 30 minutes, our hot box requires 6-8 hours to heat the asphalt prior to use.

The Bagela recycler has a 7-tonne capacity which is enough for a full day's work when patching potholes by hand and can also produce enough hot mix asphalt to repair service cuts during the winter months.

Based on staffs experience with the Bagela recycler they are confident that the purchase, if approved, will show a significant increase in operational efficiency with respect to response time and number of potholes that can be patched during a workday.



**FINANCIAL IMPLICATIONS:** Funding for the purchase will be \$20,900 (25%) from the Water Utility Depreciation Fund and \$62,600 (75%) from the Operating Reserve – Unrestricted.

**COMMUNITY ENGAGEMENT:** When the equipment is received and put into a service staff will prepare a media release showing the equipment in use.

**ENVIRONMENTAL IMPLICATIONS:** There will be a significant reduction in greenhouse gas emissions as compared to using the existing asphalt hot box to recycle asphalt.

**SOCIAL JUSTICE IMPLICATIONS:** There are no social justice implications associated with the purchase of this equipment..

**ALTERNATIVES:** Do not approve the purchase of the asphalt recycler at this time.

**ATTACHMENTS:** None

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Report prepared by: Aaron Bourgeois, Director of Operations  
Report and Financial approved by:

# SYNOPSIS

## Territorial Acknowledgement

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As the Town is making continual efforts to improve all aspects of Diversity, Equity and Inclusions in all of our functions, it was brought to our attention that the current territorial acknowledgement used at Council meetings could be improved upon.

The current territorial acknowledgement used for Council meetings is **“We [I] would like to begin by acknowledging that the land on which we gather is the traditional unceded territory of the Mi’kmaw Peoples.”**

The territorial acknowledgement used at the Diversity, Inclusion and Equity Committee is more inclusive and better represents more current practices in this regard. The territorial acknowledgement is **“I would like to acknowledge that our gathering today is taking place in (MEEG-MA-GEE), the traditional, unceded and ancestral territory of the Mi’kmaw people. I would also like to acknowledge that Nova Scotia has another unique people. These are the Indigenous Blacks of Nova Scotia whose legacy and contributions date back over 400 + years predating confederation of this land. We are all treaty people.”**

The proposed change is more inclusive and better represents current practices in the area of diversity, inclusion and equity.

### **MOTION:**

**That Council approve changes to the territorial acknowledgement.**



## AMHERST TOWN COUNCIL

RFD# 2022078

Date: November 28, 2022

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**TO:** Mayor Kogon and Members of Council

**SUBMITTED BY:** Jason MacDonald, CAO

**DATE:** November 28, 2022

**SUBJECT:** Territorial Acknowledgement

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**ORIGIN:** Continual efforts to improve inclusion, diversity and equity in all of our functions and operations.

**RECOMMENDATION:** That Council approve changes to the territorial acknowledgement.

**BACKGROUND:** The current territorial acknowledgement used for Council meetings is “**We [I] would like to begin by acknowledging that the land on which we gather is the traditional unceded territory of the Mi’kmaw Peoples.**”

**DISCUSSION:** The territorial acknowledgement used at the Diversity, Inclusion and Equity Committee is more inclusive and better represents more current practices in this regard. The territorial acknowledgement is “**I would like to acknowledge that our gathering today is taking place in (MEEG-MA-GEE), the traditional, unceded and ancestral territory of the Mi’kmaw people. I would also like to acknowledge that Nova Scotia has another unique people. These are the Indigenous Blacks of Nova Scotia whose legacy and contributions date back over 400 + years predating confederation of this land. We are all treaty people.**”

**FINANCIAL IMPLICATIONS:** There are no financial implications to this decision.

**SOCIAL JUSTICE IMPLICATIONS:** The proposed change is more inclusive and better represents current practices in the area of diversity, inclusion and equity.

**ENVIRONMENTAL IMPLICATIONS:** There are no environmental implications to this decision.

**COMMUNITY ENGAGEMENT:** A media release will be issued once a decision of Council has been made.

**ALTERNATIVES:** Do not change the territorial acknowledgement.

**ATTACHMENTS:** None.



# SYNOPSIS

## Special Election Date

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Sheila Christie's last day on Council was October 31, 2022. The Municipal Elections Act states that Council must choose a date for a special election within 4 weeks of a vacancy. *Sec 13 (2) The day fixed for the special election shall be a Saturday not more than eleven weeks after the meeting of the council at which the day was named.*

Per the legislation, Council has 4 weeks from the date of a vacancy to fix a date. The timeline is as follows:

November 1, 2022 –	Date of Vacancy
November 28, 2022 -	Regular Council Meeting – 4 weeks from vacancy. Council <b>must</b> approve a date for the election on this day at the latest.
December 27, 2022 -	1 <sup>st</sup> ad issued for Call for Nominations
January 10, 2023 -	2 <sup>nd</sup> ad issued for Call for Nominations
January 17, 2023 -	Nomination Day
February 11, 2023 -	Election Day

### **MOTION:**

**That a special election be held on Saturday, February 11, 2023 to fill the current vacancy on Council.**

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**TO:** Mayor Kogon and Members of Council

**SUBMITTED BY:** Kimberlee Jones, Returning Officer

**DATE:** November 28, 2022

**SUBJECT:** Special Election Date

---

**ORIGIN:** Special Election due to the resignation of Councillor Christie

**LEGISLATIVE AUTHORITY:** Section 13 (1) of the *Municipal Elections Act* states: Subject to subsection (8), within four weeks after a vacancy occurs on a council because (a) an insufficient number of candidates are nominated to fill the vacancies at a regular or special election; or (b) a councillor dies, resigns, becomes disqualified or forfeits office, the council or, where there is no council, the Minister shall name a day for a special election to fill the vacancy and, if no regular meeting of the council is to be held within that time, the clerk shall call a special meeting for the purpose.

**RECOMMENDATION:** That a special election be held on Saturday, February 11, 2023 to fill the vacancy on Council due to the resignation of Sheila Christie.

**BACKGROUND:** Sheila Christie's last day on Council was October 31, 2022. The Municipal Elections Act states that Council must choose a date for a special election within 4 weeks of the vacancy. Sec 13 (2) *The day fixed for the special election shall be a Saturday not more than eleven weeks after the meeting of the council at which the day was named.*

**DISCUSSION:** Per the legislation, Council has 4 weeks from the date of a vacancy to fix a date. The timeline is as follows:

- November 1, 2022 – Date of Vacancy
- November 28, 2022 - Regular Council Meeting – 4 weeks from vacancy. Council **must** approve a date for the election on this day at the latest.
- December 27, 2022 - 1<sup>st</sup> ad issued for Call for Nominations
- January 10, 2023 - 2<sup>nd</sup> ad issued for Call for Nominations
- January 17, 2023 - Nomination Day
- February 11, 2023 - Election Day

**FINANCIAL IMPLICATIONS:** As this was unexpected it is not included in the 2022/23 operating budget. It is expected to cost approximately \$30,000 and will be funded from the operating reserves.





**AMHERST TOWN COUNCIL**

**RFD# 2022083**

**Date: November 28, 2022**

**SOCIAL JUSTICE IMPLICATIONS:** There are none

**ENVIRONMENTAL IMPLICATIONS:** There are none

**COMMUNITY ENGAGEMENT:** In the new year, details of the election process will be heavily promoted

**ALTERNATIVES:** There are no alternatives. The Town must hold a special election

**ATTACHMENTS:**

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Report prepared by:

Report and Financial approved by:



# SYNOPSIS

## Alternative Voting

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In 2018 Council approved the Alternative Voting Bylaw. In 2020, with the COVID situation and multiple lockdowns and restrictions, Council authorized a fully alternative voting election process. This saw voting done exclusively by telephone or internet with no paper ballots.

The special pricing agreement that was brokered through the AMA for the Fall 2020 elections is still in effect with Intelivote Systems. The 2020 election process was extremely favorable with increased voter turn out. Bringing people physically out to a polling station in mid February is problematic due to winter weather conditions.

This system allows people to vote from anywhere with a phone or internet line.

### **MOTION:**

**That Council approve of the proposed alternative voting solution of a full electronic voting process of telephone and internet voting for the February 2023 Special Election**

---

**TO:** Mayor Kogon and Members of Council

**SUBMITTED BY:** Kimberlee Jones, Returning Officer

**DATE:** November 28, 2022

**SUBJECT:** Alternative Voting

---

**ORIGIN:** Special Election due to the resignation of Councillor Christie

**LEGISLATIVE AUTHORITY:** *Municipal Elections Act*. R.S., c. 300, s. 1. **Sec 146A (1)** A council may by by-law authorize voters to vote by mail, electronically or by another voting method.

Alternative Voting Bylaw A-7

**RECOMMENDATION:** That Council approve of the proposed alternative voting solution of a full electronic voting process.

**BACKGROUND:** In 2018 Council approved the Alternative Voting Bylaw. In 2020, with the COVID situation and multiple lockdowns and restrictions, Council authorized a fully alternative voting election process. This saw voting done exclusively by telephone or internet with no paper ballots.

**DISCUSSION:** . The special pricing agreement that was brokered through the AMA for the Fall 2020 elections is still in effect with Intelivote Systems. The 2020 election process was extremely favorable with increased voter turn out. Bringing people physically out to a polling station in mid February is problematic due to winter weather conditions. This system allows people to vote from anywhere with a phone or internet line.

**FINANCIAL IMPLICATIONS:** As this was unexpected it is not included in the 2022/23 operating budget. It is expected to cost approximately \$30,000 and will be funded from the operating reserves.

**SOCIAL JUSTICE IMPLICATIONS:** Online and telephone voting is more accessible to the general public than physically travelling to the polls, especially in winter. It is also more likely to better engage younger and marginalized voters who traditionally have lower voter turnout.

**ENVIRONMENTAL IMPLICATIONS:** An all electronic election will benefit the environment. No paper ballots, disposable ballot boxes, less emissions as people are not driving to the polls. As well, polling stations would require significant PPE, such as chemical sanitizers, disposable gloves, and masks which will end up in a landfill.





**AMHERST TOWN COUNCIL**

**RFD# 20225084**

**Date: November 28, 2022**

**COMMUNITY ENGAGEMENT:** In the new year, details of the election process will be heavily promoted.

**ALTERNATIVES:** 1) Do not use alternative voting.

**ATTACHMENTS:**

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Report prepared by:

Report and Financial approved by:



# SYNOPSIS

## Use of Permanent Register of Electors

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Section 30 of the *Municipal Elections Act* bestows municipal council with the legislative authority to select the process of establishing the voters list by either conducting an enumeration or using any permanent register of electors established and maintained for use in a federal or provincial election, as the basis for the preliminary list of electors for all or part of the municipality.

Research has shown that, with electronic databases such as through federal and provincial departments, lists can be maintained faster and more efficiently. Since at least 1997 we have procured a permanent voters list from Elections Canada. It is provided in a format that is easy to use and adapt to our voting districts.

### **MOTION:**

**That Council approve the use of the Permanent List of Electors for the upcoming Municipal Elections and authorize the Chief Administrative Officer or Municipal Clerk to enter into an agreement with Elections Canada to obtain said list.**

---

**TO:** Mayor Kogon and Members of Council

**SUBMITTED BY:** Kimberlee Jones, Returning Officer

**DATE:** November 28, 2022

**SUBJECT:** Use of the Permanent Register of Electors

---

**ORIGIN:** Special Election due to the resignation of Councillor Christie

**LEGISLATIVE AUTHORITY:** Section 30 of the *Municipal Elections Act* bestows municipal council with the legislative authority to select the process of establishing the voters list by either conducting an enumeration or using any permanent register of electors established and maintained for use in a federal or provincial election, as the basis for the preliminary list of electors for all or part of the municipality.

**RECOMMENDATION:** That Council approve the use of the Permanent List of Electors for the upcoming Municipal Elections and authorize the Chief Administrative Officer or Municipal Clerk to enter into an agreement with Elections Canada to obtain said list.

**BACKGROUND:** The Town of Amherst has not conducted an enumeration for over 20 years. Enumerations are expensive, labor intensive and research has shown that, with electronic databases such as through federal and provincial departments, lists can be maintained faster and more efficiently. Since at least 1997 we have procured a permanent voters list from Elections Canada. It is provided in a format that is easy to use and adapt to our voting districts.

**DISCUSSION:** As mentioned above, staff has used this permanent list for all Town elections since 1997. The Town enters into a sharing agreement with Elections Canada. In exchange for the use of the permanent list, the Returning Officer agrees to return a revised, updated list to Elections Canada following the election.

**FINANCIAL IMPLICATIONS:** There are no financial implications to this recommendation

**SOCIAL JUSTICE IMPLICATIONS:** There are none

**ENVIRONMENTAL IMPLICATIONS:** There are none

**COMMUNITY ENGAGEMENT:** As in previous elections, eligible voters will receive Voter Notification by mail with directions on Election details.



**AMHERST TOWN COUNCIL**

**RFD# 2022085**

**Date: November 28, 2022**

**ALTERNATIVES:** There are no reasonable alternatives. A full enumeration process is costly and time consuming. With a shorter timeframe due to a special election, staff cannot coordinate and effective enumeration

**ATTACHMENTS:**

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Report prepared by:

Report and Financial approved by:



# SYNOPSIS

## December Meeting Dates

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The Town's Proceedings of Council Policy sets the third and fourth Mondays of the month for the Committee of the Whole and Council meetings, respectively. The Policy also permits the rescheduling of such meetings by resolution of Council.

The regular December Committee of the Whole and Council meetings are scheduled for the 3<sup>rd</sup> and 4<sup>th</sup> Mondays, which would be December 19<sup>th</sup> and December 26<sup>th</sup>.

Due to the Christmas holidays, it is recommended that these meetings be rescheduled to hold Committee of the Whole on it's regular day, December 19<sup>th</sup>, with a start time of 3:00 p.m. instead of 4:00 p.m., and that Council be rescheduled from the 26<sup>th</sup> to the 19<sup>th</sup> at 6:00 p.m.

### **MOTION:**

**That due to the Christmas holidays, the December 19, 2022 Committee of the Whole meeting be rescheduled to begin at 3:00 p.m. instead of 4:00 p.m., and that the regular December Council meeting be rescheduled from December 26<sup>th</sup> to December 19<sup>th</sup>, 2022 at 6:00 p.m.**



**AMHERST TOWN COUNCIL**

**RFD# 2022074**

**Date: November 28, 2022**

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**TO:** Mayor Kogon and Members of Council

**SUBMITTED BY:** Kimberlee Jones, Director Corporate Communications / Municipal Clerk

**DATE:** November 28, 2022

**SUBJECT:** December Meeting Dates

---

**ORIGIN:** 2022 Calendar – Christmas Holiday

**LEGISLATIVE AUTHORITY:** Proceedings of Council Policy 10350-24 Section 5 states “Regular meetings of Council or Committee of the Whole may be rescheduled, relocated or cancelled: (b) by Resolution of Committee of the Whole at a meeting three or more days in advance of the additional or special meeting”

**RECOMMENDATION:** That due to the Christmas holidays, Council reschedule the December Committee of the Whole to begin at 3:00p.m., and further reschedule the regular December Council meeting from December 26<sup>th</sup> to December 19<sup>th</sup> at 6:00 p.m.

**BACKGROUND:** The Town’s Proceedings of Council Policy sets the third and fourth Mondays of the month for the Committee of the Whole and Council meetings, respectively. The Policy also permits the rescheduling of such meetings by resolution of Council.

**DISCUSSION:** The regular Council meeting date for December would be December 26<sup>th</sup>. By holding COW at 3pm-5pm on the 19<sup>th</sup>, this allows staff an hour to turn out a Council agenda package for a 6pm meeting.

Due to the Christmas holidays, it is recommended that this meeting be rescheduled to hold Committee of the Whole on December 19, 2022 at 3 p.m. with Council to follow that same day at 6:00pm.

**FINANCIAL IMPLICATIONS:** There are no financial implications to changing the meeting dates.

**COMMUNITY ENGAGEMENT:** The public and media will be notified of any changes by media release.

**ENVIRONMENTAL IMPLICATIONS:** No environmental implications.

**SOCIAL JUSTICE IMPLICATIONS:** There are no social justice implications to rescheduling the meetings.



**ALTERNATIVES:** Schedule alternate dates. This is not recommended as scheduling meetings over the holidays is extremely difficult.

**ATTACHMENTS:** N/A

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Report prepared by: Natalie LeBlanc, Deputy Clerk  
Report and Financial approved by:

# **Internal Committee Report**

## **Planning Advisory Committee**

**November 2022**

---

The Planning Advisory Committee met November 7<sup>th</sup> at 4:00 p.m. in Town Hall Council Chambers.

The consultants from Upland Planning & Design were in attendance to discuss various subjects relating to the draft Land Use Bylaw. A draft Municipal Planning Strategy and Land Use Bylaw are expected from the consultant by the end of November.

# **Internal Committee Report**

## **Audit Committee**

### **November 2022**

---

The Audit Committee met on November 8, 2022 at 4:00 p.m. in Town Hall Council Chambers.

The Town of Amherst consolidated financial reports were presented, with a motion passed to refer them to Council for approval. Council gave approval at a Special meeting on November 9<sup>th</sup>.

The Committee also reviewed the Second Quarter Financial Report and Capital Budget Actuals-to-Date.

# **Internal Committee Report**

## **Amherst Youth Town Council**

### **November 2022**

---

Amherst Youth Town Council (AYTC) met on October 17, 2022, and twice during November, once on November 7th and once on November 22nd. At these meetings we focused our discussions on elections of officers, setting our goals and objectives for the year and determining what future activities to be part of.

Our goals for the year center around three main points: advisory, awareness and activities. Stepping into our role as an active advisory body for the Amherst Town Council will include us making it a priority to attend every committee of the whole meeting. We want to be more aware of the Amherst Town Councils decisions and discussions so that we can productively advise you on what is important to youth.

We are also looking to raise awareness about the AYTC with youth, with the intention of filling our open positions.

We have discussed youth issues we think are important and have decided to look into youth hunger. We plan to meet with the staff at local schools to ask about their student food pantries and also meet with the after the school bell program, a group who provides kids with food to take home on the weekends.

We also plan on holding a skating event at this year's winter carnival.

Finally, the AYTC also participated in many of the holiday events this past month including the Christmas Parade, Light-Up and Boston Tree send off.

# **Internal Committee Report**

## **Accessibility Advisory Committee**

**November 2022**

---

The Accessibility Advisory Committee met on November 9, 2022 at 6:00 p.m. in Town Hall Council Chambers.

The Community Well-being Manager Sharon Bristol presented the updated draft Accessibility Strategy, noting this is intended to be a living document that will be reviewed and updated going forward. Survey results were also discussed as well as the Action Plan, how it will be implemented and the ongoing monitoring and evaluation. The Committee was asked to email Sharon with any suggestions and/or corrections so the draft Strategy can be presented to Council for consideration in December.

An update on the stadium elevator was provided. Staff indicated that repairs have been delayed due to the difficulty in receiving the needed parts however they are expecting it will be functional by mid-December.

# External Committee Report

## Cumberland Public Libraries

November 2022

---

### **Annual Book Sale**

The Library's Annual big book sale starts on Monday, November 28<sup>th</sup> and will run until Saturday, December 10<sup>th</sup>. Proceeds from the book sale go towards our Summer Reading Club 2023. Last year's county wide book sale raised over \$1600.

### **Living Wage Increase**

The Board approved a wage increase to go into effect April 1, 2023 to work toward a living wage. Currently our entry level positions start at \$13.60/hour, this will increase to \$15.00. Other library regions in Nova Scotia have their entry level positions starting at \$17.00 and up. The Canadian Centre for Policy Alternatives estimates a living wage in Cumberland County is currently \$20.40.

### **Circulation Statistics**

We are very excited to see an increase in our circulation thanks to Same Page. From April – October 2022, we have circulated over 80,000 items. In 2020-21 Cumberland Public Libraries circulated a total of 102,349. Circulation includes online resources like: Overdrive, Hoopla, e-magazines, accessible circulation, as well physical items.

### **Ongoing Building Issues**

The Amherst Library building continues to experience issues with the HVAC system. This causes leaks and maintenance often means that the library has to close to the public. The heating system was installed when the building was renovated in the late 1980s and the Town replaced several heat pumps a few years ago. As the remaining heat pumps are now causing some issues the Chief Librarian and the Town CAO toured the building and the CAO advised that repairs will be included in the upcoming capital budget for Council consideration.

**The next Board meeting is February 16, 2023.**

# External Committee Report

## YMCA of Cumberland

November 2022

---

### **Early Learning Centre**

Friday Night Fun, in partnership with the Town of Amherst is ongoing.

A Babysitting Course was held on October 29, 2022

### **Membership**

Currently, we have 1189 active members.

### **Aquatics**

Early afternoon Aquafit classes are averaging 20 participants each day.

### **Community Development**

We ran the “Set it and Forget It” crockpot cooking class for seniors. The class booked up fast and we are looking to do a second run of the program.

Community Kitchen runs each Wednesday. This program provides a hot meal free of charge or by goodwill offering.

The YMCA of Cumberland is a drop-off location for Coats for Cumberland which provides gently used winter outerwear to those in need, in all sizes. Coats and winter outerwear were available for selection at the Amherst Centre Mall on November 18, 2022 from 11:00 a.m. - 2:00 p.m.

The Santas for Seniors tree is up in the lobby. The program provides Christmas gifts for seniors in our community. Members of the public are invited to select a gift tag - purchase the item on the tag and return it, unwrapped (or in a gift bag), to the YMCA by December 16, 2022. Donations of gift bags, tissue paper and holiday cards are also accepted.

### **Donations/Fundraising**

Planning for Coldest Night of the Year, taking place on February 25, 2023 has started. The campaign launch date is December 1<sup>st</sup>.

# External Committee Report

## L.A. Animal Shelter

### November 2022

---

The shelter continues to run at full capacity with approximately 80 cats and kittens. The animal turnover is high with many adoptions every week, but cats and kittens are being surrendered at a very high rate. We also have 7 dogs in care - more than we have had in quite some time.

We continue to move forward with plans for fundraising for a new building. Committees have been struck and we hope to start working with community members soon to begin the process.

The fundraiser with Two Fools Productions was a great success. Approximately \$13,000 was raised over 10 nights.

Last weekend we attended and were the beneficiary of the Third Annual Phyllis Blummel Memorial Bazaar. Another successful event!

We will be participating in the Christmas Extravaganza at Dayle's Market in December.

# External Committee Report

## Senior Safety

**November 2022**

---

The Cumberland Senior Safety Committee met on November 8<sup>th</sup> from 1:30 to 3:00 at the Amherst Police Department Community Room.

The Senior Safety Coordinator had applied for a grant for \$1000 to cover making emergency kits for seniors. These will hold typical emergency supplies for winter including flashlights, can openers, warm mittens and other emergency supplies needed for safety while in their homes during a power outage.

In October the coordinator made 9 home visits and took part in 5 events. She has also been working with local libraries to have a place that seniors can come to so she can go over government forms with them and help them apply for any assistance.

The events included: The Fall Fair at Holy Family Church, Library participation in Parrsboro and River Hebert, at table at the Amherst Farmers' Market and the grant presentation at Amherst Town Hall. These events have led to 68 phone calls for help in applying for assistance from seniors, as well as 6 new clients and 9 more referrals from Continuing Care.

There is a Seniors Care Grant that has been created by the government of Nova Scotia to help seniors stay in their own homes. A lot of calls have to do with this assistance and how to apply. This grant will help with snow shovelling and home tidiness. Not to mention home heating assistance and Continuing Care assistance being some of the forms that seniors need help filing.

Next Meeting: December 13<sup>th</sup> at 1:30.

# External Committee Report

## Advisory Committee to Reduce Poverty

November 2022

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The Advisory Committee to Reduce Poverty met on November 17, 2022 at 4:30 p.m. in Town Hall Council Chambers. Deputy Mayor Landry welcomed guests from the Amherst Board of Police Commissioners, Chair Sandy Fairbanks and Chief of Police Dwayne Pike, as well as Aiden Kivisto, Manager Community Development YMCA of Cumberland to the meeting.

A presentation was given by Chief Pike on the current state of policing and homelessness. His presentation identified that homelessness is not a new issue to policing but rather one they have been working with for some time. He shared statistics on the number of calls relating to persons living rough and identified several causes and issues. The committee also heard a presentation from Aiden Kivisto, Manager of Community Engagement at the YMCA of Cumberland on the proposed service-based count that is scheduled to happen in March 2023. This will be a follow up to the 2021 count which identified over 30 individuals who were homeless or experiencing housing insecurity in our community. He also noted that a survey done in June 2022 identified 10 youth aged 14-20 who self-identified as homeless or couch surfing. Aiden informed the committee that the Service Based Count Advisory Committee is seeking additional membership. He can be reached via the YMCA of Cumberland.

Councilor Houghtaling reported on the chair's meetings with Mitacs. Mitacs is an organization that partners with others to provide co-funding for research projects. Working with municipalities initiated within the last year and the potential to develop academic based studies is an exciting opportunity. An overview on how Mitacs funding might help identify quantitative and qualitative information around the scope of poverty in our community. Two key objectives were identified.

1. Understanding the barriers to economic resiliency within the specific context of the unique challenges faced within this specific geography of Nova Scotia.

2. What can be done to reduce these barriers or build better pathways out of poverty and towards economic resiliency?

These would be open to project scopes that would align with Housing, Food Security, Youth Engagement/Outreach pillars as identified in the inter municipal poverty reduction strategy development currently underway. A meeting that Councillor Houghtaling and I had regarding a potential project around rural transportation service was also briefly discussed. Three separate academics have been identified as potential partners thus far. Once all discussions have taken place, the options will be presented to the committee and a decision on how to proceed will be made at that time.

An update on the Winter Emergency Temporary Shelter was given:

- Funding is still pending as the WETS proposal team confers with DCS.
- Policies and procedures are nearing completion – emphasis is being placed on security and safety.
- Public concerns continue to be addressed by the committee and community members have been able to attend meetings to voice their concerns directly.
- NSCC is developing a social media campaign, and Ann Keddy has taken on public communications.

A request from Cornerstone to transfer the deed at 8/10 Prince Arthur was reviewed. A motion was made by Councilor Jennifer Houghtaling, seconded by Councilor Carla Black which reads as follows:

*“I recommend to Amherst Town Council that the Town of Amherst transfer ownership of the subject property, 8/10 Prince Arthur St, to Cornerstone for use as a housing hub including affordable housing units, emergency traditional housing beds, housing support meeting space and other such similar uses, subject to a purchase and sale agreement to be negotiated between Cornerstone and the Town of Amherst, and further, that the Municipality of the County of Cumberland be recommended to be supportive of potential future financial support requests by Cornerstone for this project, or affordable housing support in the region.”*

The draft strategic plan Goals and Action items section were discussed and consensus agreed that these would form the basis of work to be completed over the next 1-3 years. The final document will be brought to the next PRAC meeting for approval. The intent is to have the Strategy be presented to respective councils in January. The action items will be reformulated into short term and longer-term goals for the purpose of defining the scope of work to be required immediately. Community engagement and quarterly reporting will be included in the document to measure success. The document will be amended with quarterly reports seeking to enable the 'living document' to be revised as needed.

A meeting has been scheduled with the NSCC to discuss partnerships in developing a youth tutoring program.

A roundtable discussion on Youth engagement opportunities was discussed and many great ideas for community collaboration were discussed such as:

- Youth outreach within the school system.
- Integrating AYTC and the newly established County of Cumberland Youth Council with anti-poverty endeavours.
- Food pantries extending into the county via the school systems.
- Integrating outreach and education across the various age groups within our youth and promoting a culture of openness and acceptance at an early age.

Shelley Hoeg-Eaton reported that the new Cumberland County representative will be Rachel Sovka from Parrsboro.

# External Committee Report

## Municipal Alcohol Project

November 2022

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The Municipal Alcohol Project Committee met on November 1st from 1:30 to 2:30. I was in Halifax but was able to attend via zoom.

**Alcohol and Key Messages around Cancer:** The draft messaging is almost ready and will be useful once the new guidance on health and alcohol are released sometime in January 2023. The Canadian Cancer Society is preparing a brief to show the risk of alcohol consumption as it relates to various cancers to be shared at the same time as the new health guidance comes out in January 2023. Nova Scotia Health will review the key messages and look to have the more current messaging on their website after January.

**Communication Campaign for MAP:** This Committee will send a proclamation to all three municipalities to be read once the new guidelines have been released. There will also be radio spots released through Communications Nova Scotia. We will be looking to do a presentation to all three municipalities next year as well.

**Winter Holiday Messaging:** There will be a letter sent to all three municipalities thanking them for hosting family friendly (alcohol free) events during the holiday season. We will also re-share the original alcohol during the holiday social media posts from last year and try to prepare some new posts.

### **Cumberland Homelessness and Housing Support Association: Cornerstone**

There was a housing forum held in October that was well attended. Mental Illness and addiction including alcohol addiction were mentioned as barriers to housing, which is information that was also reflected in the Service Base Count that was done last year. There were good observations by members of this Committee that participated in the forum. One of the most valuable and impactful discussions was hearing the firsthand accounts of what those living in housing insecurity are experiencing.

**Alcohol Warning Labels:** The Canadian Alcohol Policy Evaluation Network was successful in encouraging partners, including health partners, to write letters of support for Ireland's push for the Public Health Alcohol Labelling Regulations to be implemented. These are very strong labelling regulations for alcohol. Many countries, including Canada, have seen an upsurge of support for this. Nova Scotia has a few sentinels working to prepare for this future of labelling including a web-petition that is being finalized and will be piloted in the near future. M-16 is the Canada National Warning Label Strategy for Alcohol products that was put forward in June of 2022. There is now new momentum for Cancer Causing labels in Canada.

**3rd Nova Scotia Alcohol Forum:** This is being prepared for Spring of 2023 and there is a small group already working on the planning of this event.

**Next Meeting:** December 6th from 1:30 to 3:00 pm.

# **External Committee Report**

## **Northern Region Solid Waste Resource Committee**

### **November 2022**

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#### **C&D Regulation Changes**

Nova Scotia Environment and Climate Change held targeted engagements sessions on the draft C&D guidelines on November 16. As you may recall, treated wood will be banned from disposal at C&D facilities effective July 2023. During this meeting the department also shared the proposed amendments to the C&D guidelines.

#### **EPR Summit**

Divert NS is holding a summit on Municipal Planning for Printed Package and Paper on December 6<sup>th</sup> in Dartmouth. Municipalities are looking forward to this as there will be presentations from municipal representatives from BC and Ontario who have already gone through the negotiation process with the PPP Industry.

# External Committee Report

## GFL Cumberland Central Landfill Community Liaison Committee

November 2022

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This new Committee met on November 9<sup>th</sup> in the Town of Amherst board room from 11:00 to 1:00. Lisa Emery is the council representative on this committee that takes over from the former CJSMA that operated the landfill. This committee will also have community members from the area around the landfill.

There was a review of the Terms of Reference for the committee. One of the key items from the TOR is that the Chairperson or Facilitator will be a neutral person (community member) and cannot represent a vested interest with respect to the site. The committee will meet twice a year (Spring and Fall) and will receive reports with respect to operations and environmental monitoring during these meetings.

**Initiatives:** There is some learning underway on how to operate the septage facility which was operated by the Municipality of Cumberland prior to GFL purchasing the landfill site. There is a project underway to install a remote terminal for use by the septage haulers after hours. Work is expected to be completed December 31, 2022.

Various electrical components of the landfill leachate collection system are being upgraded. Work will be completed by December 31, 2022.

A new compost screener has been purchased and has dramatically improved the compost.

**Operations:** A second excavator was purchased in April of 2022. Unfortunately, it caught fire in October and will have to be written off.

Dillion Consulting has conducted its Spring ground and surface water monitoring sampling and reporting around the site. They also conducted the Fall monitoring in October, which will be presented next Spring.

Incoming Material by the tonne: Waste: 7,112; Recycling: 1,965; Organics: 3,468; C&D: 5,150; Asbestos: 305; Septage: 5,234 and Bio-solids: 2,385.

The Nova Scotia Department of Environment and Climate Change has released new construction and demolition disposal guidelines that will come into effect July 1, 2023. Treated wood will be banned from C&D disposal facilities and will need to be disposed of in a second generation landfill as garbage or municipal solid waste. This will require a bylaw change by municipalities involved, and will bring more costs to those that work in construction.

**Spring Monitoring Report by Dillon:** Dillon has been the consultant for the Cumberland Central Landfill since the 1990s as far as engineering and environmental monitoring services. GFL Environmental Inc took over the landfill on February 1, 2022 and Dillon has continued to provide the monitoring services.

Summary of Surface Water Results: Concentrations of aluminum, iron and manganese are consistent with previous results. Concentrations of Chloride were at higher levels but consistent with previous results.

Summary of Ground Water Results: The elevations were consistent with previous reports except for one spot where they were marginally higher. There was one cell with higher turbidity and colour from iron. Potential changes will be confirmed during the Fall monitoring event.

On-site Wells: e.coli was not detected in the wells. The production well had higher iron, manganese and turbidity. Same result for the recycling trailer and scale house well. The water from these wells is not consumed and signs are posted.

Next meeting will be in the Spring of 2023 before June.

# **External Committee Report**

## **Cumberland Joint Services Management Authority**

**November 2022**

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The Committee met on November 26<sup>th</sup> at 6:30 pm in Amherst Town Hall Council Chambers.

The draft audited financial statements were presented by Denise Smith of Jorgensen & Bickerton Inc. These statements were approved as presented and will be sent to the three municipalities to include in their financial reports to their respective councils.

The next meeting will be some time in the new year. This will be a final meeting to wind up the society of CJSMA.